

**ROYAL BOROUGH OF WINDSOR & MAIDENHEAD
PLANNING COMMITTEE**

WINDSOR RURAL DEVELOPMENT CONTROL PANEL

1 June 2016

Item: 1

Application No.:	16/00266/FULL
Location:	Land At Hill House Cross Road Sunningdale Ascot
Proposal:	Erection of 4 x apartments (3 x 2 bed and 1x 3 bed).
Applicant:	Kebbell Developments Ltd
Agent:	Mr Paul Dickinson- Paul Dickinson And Associates
Parish/Ward:	Sunningdale Parish
If you have a question about this report, please contact: Alistair De Joux on 01628 685729 or at alistair.dejoux@rbwm.gov.uk	

Application No.:	16/01179/FULL
Location:	Land At Hill House Cross Road Sunningdale Ascot
Proposal:	Erection of 5 x apartments with associated works
Applicant:	Mr Mills - Kebbell Developments Ltd
Agent:	Mr Paul Dickinson - Paul Dickinson And Associates
Parish/Ward:	Sunningdale Parish
If you have a question about this report, please contact: Alistair De Joux on 01628 685729 or at alistair.dejoux@rbwm.gov.uk	

1. SUMMARY

- 1.1 This report deals with two current applications for apartments at the same site. Application 1 is for four apartments and the more recent Application 2 is for five.
- 1.2 The two applications follow three previous applications for apartments and, prior to that, for a single house on the same site. The single house proposal was approved and remains extant, while all of the apartment proposals have been refused. Two of these refusals were the subject of recent appeals, which were considered concurrently. Like the current pair of applications, the dismissed recent appeals were also for apartment buildings, one to accommodate four flats (Appeal A) and the other for five (Appeal B). Both were refused for a range of reasons but the Inspector dismissed them primarily on just one issue. This was the impacts to the future health and viability of a protected oak tree to the rear of the proposed building. For Appeal B only, the Inspector did not consider that the Council's approach in regard to provision of mitigation for the Thames Basin Heaths SPA was robust. The other reasons for refusal were considered to be overcome by the appeal Inspector, so the consideration below is largely limited to the points on which the appeals were dismissed.
- 1.3 The site of the proposed building comprises a tennis court and adjoining garden land within the grounds of Hill House, including a woodland garden on the rear part of the site. Trees here are protected by TPO, and include the English oak referred to above together with a mix of native and non-native pine species. Other trees within the garden of Hill House, to the east of the application site, are also covered by TPO.
- 1.4 The site is within a 'leafy residential suburbs' townscape character area as defined by the Council's Townscape Assessment. Neighbourhood Plan Policy NP/DG1 supports single detached dwellings in this character area, unless it can be satisfactorily demonstrated that other forms of development would retain the identified character of the area (Policy NP/DG1.2) or where it can be demonstrated that the proposed development would not harm local character (Policy NP/DG1.2). The intensification of the use of the site that would result from apartment development of the type proposed here formed part of the basis for refusal of the previous applications, but this was not upheld by the Inspector at the subsequent appeals.

- 1.5 It is understood that Application 1 is now subject to an appeal to the Planning Inspectorate on grounds of non-determination, although at the time of writing this report no start letter for the appeal had been received by the Council.

Application 1: 16/00266/FULL

It is recommended the Panel grants planning permission subject to the conditions listed in Section 9 of this report OR, if the appeal is registered before a decision is made, that a 'would have approved' decision be noted.

Application 2: 16/01179/FULL

It is recommended the Panel authorises the Borough Planning Manager:

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|----|---|
| 1. | To grant planning permission subject to demonstrating that the roof terrace at Apartment 5 will not result in loss of privacy to neighbours and with the conditions listed in Section 9 of this report. |
| 2. | To refuse planning permission if drawings to demonstrate that the roof terrace at Apartment 5 will not result in loss of privacy to neighbours have not been provided by 1st June 2016, for the reason that the would result in an unacceptable loss of residential amenity to neighbouring properties. |

2. REASON FOR PANEL DETERMINATION

- The Council's Constitution does not give the Borough Planning Manager delegated powers to determine the application in the way recommended; such decisions can only be made by the Panel.

3. DESCRIPTION OF THE SITE AND ITS SURROUNDINGS

- 3.1 The site is part of the larger Hill House property, which is located on the northern side of Cross Road. It consists of a tennis court and ground around it and to the rear including woodland garden land, all of which currently forms part of the extensive garden at Hill House. Hill House itself is a two-storey house of an attractive design that appears to be of late Georgian or early Victorian origin, with more recent single storey rear extensions to the rear. The attractive grounds include many large and significant trees, many of which are subject to Tree Protection Orders, and some of which are within the rear part of the application site. Apart from one Scots pine tree identified for removal in the extant permission; this application would not require the felling of any of these significant and important trees.
- 3.2 The property lies near the edge of the settlement area approximately 150m to the east of the A30 London Road, within walking distance of the shops and railway station in Sunningdale. Land to the west and north is predominately residential in character, with large dwellings and, particularly on the northern side of Cross Road, flatted developments at Hillside Park - these include Richmond House, Fisher House and Beaufort House - and Dorchester Mansions. On the opposite side of Cross Road there are a number of large detached dwellings, which include Fairways and its annex - this is located close to the Cross Road frontage - and Queenswood, with other relatively closely spaced detached house towards the A30 London Road.
- 3.3 The Sunningdale Ladies Golf Club course lies to the south-east balance beyond Hill House itself, and is within the Green Belt.
- 3.4 Apart from this nearby area of the Green Belt, the site and its immediate surroundings are classified within a "leafy residential suburb" townscape type in the Council's Townscape Assessment, specifically as character area 13E (Sunning Avenue and London Road, Sunningdale). Some nearby properties to the south-west are within the "villas in a woodland setting" townscape type, although these do not form part of the immediate context for the application site.

4. DESCRIPTION OF THE PROPOSAL AND ANY RELEVANT PLANNING HISTORY

4.1 Both proposals are for the construction of apartments with basement car parking, both in a building of very similar design to that of the extant permission for a single dwelling. The differences are as follows:

- i. The basement would be considerably larger in order to accommodate the required number of car parking spaces.
- ii. The roof of the main element of the building would be more steeply pitched to form a mansard roof with rear facing dormer windows, as compared to the approximately 45-degree pitch of the crown roof in the extant permission.
- iii. There would be one external parking space provided for delivery vehicles.
- iv. For Application 1 - the four apartment scheme - there would be two each flats on each level of accommodation (referred to as lower ground and ground floor on the submitted drawings).
- v. For Application 2 (five apartments) the provision of two each flats on each of the lower ground and ground floors would be replicated in the five apartment scheme, and in addition there would be a fifth apartment within the roofspace. Part of the roof would be cut away at the rear to provide a terrace for this flat.

There would also be some other minor changes to fenestration on the flank walls, with more windows to be provided on the flank walls on both elevations and more particularly on the north-west elevation facing towards Richmond House.

4.2 Relevant recent planning history is as follows:

Ref.	Description	Decision and Date
13/01206/FULL	Construction of a detached house.	Permitted, 15.08.2013.
14/00451/FULL	Construction of five apartments.	Refused, 06.06.2014.
14/03591/FULL	Construction of 4 no. apartments.	Refused, 10.02.2015 and dismissed at appeal.
15/01199/FULL	Construction of 1 apartment block comprising of 4 x 2 bed and 1 x 3 bed apartments.	Refused, 05.06.2015 and dismissed at appeal.

5. MAIN RELEVANT STRATEGIES AND POLICIES RELEVANT TO THE DECISION

5.1 National Planning Policy Framework Sections 6, 7, 8, 9, 10, 11 and *Decision-taking*

Royal Borough Local Plan

5.2 The main strategic planning considerations applying to the site and the associated policies are:

	Within settlement area	Highways and Parking	Protected Trees	Biodiversity	Energy efficiency
RBWM Local Plan	DG1, H10, H11	P4, T5	N6		
Neighbourhood Plan	NP/H2, NP/DG1, NP/DG2, NP/DG3 and NP/EN3	NP/T1	NP/EN2	NP/EN4	NP/DG5

Supplementary planning documents

5.3 Supplementary planning documents adopted by the Council relevant to the proposal are:

- Sustainable Design and Construction
- Planning for an Ageing Population
- Thames Basin Heaths SPD

More information on these documents can be found at:

http://www.rbwm.gov.uk/web/pp_supplementary_planning.htm

Other Local Strategies or Publications

5.4 Other Strategies or publications relevant to the proposal are:

- RBWM Townscape Assessment - view at:
http://www.rbwm.gov.uk/web_pp_supplementary_planning.htm
- RBWM Parking Strategy - view at:
http://www.rbwm.gov.uk/web_pp_supplementary_planning.htm

6. EXPLANATION OF RECOMMENDATION

6.1 The key issues for consideration in both applications are:

- i Character issues as determined in the recent appeal decisions;
- ii Impacts on the protected oak tree at the rear of the property;
- iii The mitigation of impacts on the Thames Basin Heaths SPA;
- iv impacts on protected wildlife within the site; and
- v for Application 2 only, the impacts of the roof terrace on neighbouring properties.

Impact on the character of the area

6.2 The recently appealed schemes were refused on several issues, including the following character issue which was the same in both decisions:

The intensification of the use of this site, as compared to the extant permission for a single detached house (RBWM ref. 13/01206/FULL), would result in the continued erosion of the character of this village-edge location, which is very close to the Green Belt boundary, contrary to Policies NP/DG1.1, 1.2, 1.3 and 1.6 and NP/DG3.2 of the Ascot, Sunninghill and Sunningdale Neighbourhood Plan 2011 - 2026, and advice in the National Planning Policy Framework.

6.3 For both appeals, the Inspector concluded that the apartment use would not be harmful to the character and appearance of the area as compared to the extant permission for a single house. The inspector noted that the design and siting of the building in both schemes would respect the character and appearance of the area. The decision noted the conflict with the objective of Policy NP/DG1.2 to provide for detached houses in this townscape character area, but also noted that the policy accepts that alternative forms of development may be acceptable where it can be satisfactorily demonstrated that the identified character of the area would be retained. The Inspector considered that the density of both schemes would be low, and comparable to the adjacent apartment development at Hillside Park. While there would be more vehicle movements into and out of the site as compared to an individual dwelling, the level of activity associated with the proposed use would not be so significant that it would result in harm to the tranquil environment of this part of Cross Road. The Inspector also considered that the size of the garden would be suitable for the enjoyment of future occupiers. Use by a greater number of occupants than in a single dwelling was also not considered to be likely to result in a materially greater

intensity to that of the approved scheme, and as such the tranquil character of the area would be sustained.

- 6.4 The Council's reason for refusal cited above was not therefore supported by the appeal Inspector. The two current applications are not significantly different in terms of their streetscape appearance from the appeal schemes, and the levels of activity at the site that they would generate are the same as for the previous schemes that had the same levels of accommodation. For that reason it must be considered that this issue has been satisfactorily resolved, and no objection to either of the current proposals is raised. Density was also considered, and noted as similar to the neighbouring flats.
- 6.5 Differences in the appearance from the extant permission, including the numbers of windows on flank elevations of the building, were not considered further by the Inspector as there was no objection to these points in the Council's decisions on the appealed schemes. Impact on light were also not considered in the appeal decision, but this has not been an issue objected to by the Council in any of the application for the site, and it continues to be the case that the current applications would not result in any material difference in this respect from the extant permission. The Neighbourhood Plan objection letter notes the lack of windows in the south elevation for Apartment 5 and considers that this is poor design, but this was not objected to in the application that led to Appeal B as there are sufficient other windows to ensure an appropriate level of amenity for this apartment, and no objection is raised.

Impacts on the protected oak tree at the rear of the property

- 6.6 While the appeal decisions did not consider that additional future residents at the property would result in significant detriment to the character of the area because of the intensification of activities and traffic, the decision did however agree that additional impacts on the mature Oak to the rear of the building site would result, as compared to the extant permission for a single dwelling. Both appeals were dismissed for that reason. This is a protected tree (TPO No. 17 of 1998) and is an attractive, mature tree in good condition that is visible from neighbouring properties and from Cross Road.
- 6.7 The Royal Borough of Windsor and Maidenhead Local Plan (Local Plan) Policy N6 and Policy NP/EN2 of the NP seek to protect significant trees within and outside of development sites. Policy NP/EN3 of the NP requires proposals for new dwellings on private residential gardens to have, amongst other things, an acceptable impact on the landscape and environmental value of the site. The stem diameter of the Oak tree is an indication that it is an Ancient Tree; NPPF 118 advises that planning permission should not result in the loss of aged or veteran trees found outside ancient woodland, unless the need for and benefits of the development in that location clearly outweigh the loss.
- 6.8 In both applications, the apartment building would have the same footprint as the previously approved dwelling on the site. In considering the single-house application at the site, it was accepted that a development in this location could be implemented without causing significant damage to the tree, providing appropriate precautions and controls to protect the tree including its root protection area are put in place. However, in both appeal proposals parts of the living accommodation to Flat 3 would have been closer to the Oak tree than in the approved scheme. This resulted from the infilling of a rear-facing balcony in the extant permission, which resulted in Apartment 3 having windows approximately 9 metres from the canopy of the tree, including a Juliet balcony for Bedroom 1, and the main windows serving Bedroom 2 and also a dressing room which would be built into the space where the balcony would previously have been provided. In the five apartment scheme (Appeal B) the main outlook for the roof-level Apartment 5 would have been into the canopy of the tree.
- 6.9 In this pair of applications, the balcony in the extant permission would be reinstated, and Bedroom 1 would have a pair of glazed doors to this balcony, set perpendicular to views to the oak tree and providing an alternative outlook to the rear facing windows. The rear facing windows for Bedroom 2 is in this application by another pair of glazed doors to the same balcony; these are 2.5m further from than the tree canopy than the windows in both of the Appeal proposals. In this pair of applications, the primary outlook for Bedroom 1 would now be perpendicular to the oak tree. Notwithstanding that the use of these rooms could potentially be

changed (albeit that the provision of en suite bathrooms for both bedrooms would make that less likely), it is considered that this improves the living conditions for future occupiers of Flat 3 sufficiently to make the threat of future detrimental pruning works to the tree less likely. The balcony would provide 12 sq.m of private outdoor amenity space that was absent in the two appeal schemes.

- 6.10 In Appeal B, the five unit scheme, proximity of Flat 5's rear-facing windows to the canopy of the protected oak tree was not a matter of concern for the Inspector, presumably because they would be an additional 7.5m from the tree than the windows serving the refused scheme (a total distance of almost 20m). The current Application 2 has added a large roof terrace adjacent to the lounge by cutting into the roof on this part of the building, which would however remain at full height so that the appearance of the front of the building would remain largely as in the extant permission. (This would also prevent any direct views to the west towards the adjacent flats at Hillside Court.) The addition of this terrace results in an alternative direction of outlook from Flat 5's open plan kitchen lounge in this application, and the terrace itself would provide about 45 sq.m. of outdoor amenity space for future occupiers.
- 6.11 Provision of a private balcony or terrace for Flats 3 and 5 would reduce pressure on the use of the rear garden. Plot 2 would also have a more enclosed patio area as compared to those shown in the Appeal proposals. This area of about 37 sq.m. was shown as partially enclosed with a low wall in the appeal schemes, and this wall is extended further along the back of this flat to provide a more delineated private amenity area of this flat. Both this and the other lower ground floor flat (Plot 1) would retain direct access into this garden, while and the other 'upper floor' flat, Plot 2, would have Juliet balconies overlooking the garden at a distance of approximately 12m from the oak tree. The additional amenity spaces for Plots 3 and 5 and improved delineation for the patio at Plot 2 would reduce pressure on the use of the rear garden area adjacent to the oak tree as compared to the schemes in the dismissed appeals.
- 6.12 The Tree Officer's comments take a different approach to the two applications. For Application 1, he notes that the changes that have been made in relation to Plot 3 secure a similar level of tree protection as in the previously approved scheme, and therefore he did not object to this application. For Application 2, he comments that:

The additional accommodation in the roof space together with the subdivision of the property into flats would worsen the spatial relationship between the building and the adjacent protected oak tree. This would result in more principal accommodation being occupied in close proximity to this significant and important tree and will lead to future pressure to prune it in a way that would be detrimental to its character and long term viability.

- 6.13 However, as discussed above, the additional accommodation, Plot 5, has a large outdoor terrace, and the windows facing the oak tree canopy would be approximately 20m distinct from the canopy. It is noted also that the flats are not of the type that are generally occupied by families. Greater use of the woodland area on the north side of the oak tree, which extends for another 40m north of the oak tree, could also be provided by the requiring details of sitting out areas within this area to be provided as part of the landscaping requirements in any permission that is granted. On balance, it is considered that the impacts of the proposals in regard to the protected oak tree have been sufficiently addressed in both applications to have overcome this objection in both of the refused schemes. Pruning and removal of other trees at the site including a protected oak tree were previously agreed in the extant permission.

The mitigation of impacts on the Thames Basin Heaths SPA

- 6.14 The Thames Basin Heaths Special Protection Area (SPA) was designated in 2005 to protect and manage the ecological structure and function of the area to sustain the nationally important breeding populations of Nightjar, Woodlark and Dartford Warbler. As such it has statutory protection under the Conservation of Habitats and Species (Amendment) Regulations 2012. National planning policy in respect of conserving and enhancing the natural environment is contained within NPPF 11 of and further guidance is provided within Circular 06/2005. Whilst the South East Plan has been revoked, Policy NRM6 remains in force and requires new residential development to demonstrate adequate measures to be taken to mitigate the effects of the

development on the SPA. The Council's Thames Basin Heaths Special Protection Area Supplementary Planning Document provides further guidance in respect of such mitigation.

- 6.15 The appeal site is located within the 400m - 5 km buffer zone around the SPA, where it is considered that additional residential development would result in additional recreational demands on the SPA. Since the provisions of the Community Infrastructure Levy (CIL) Regulations 2010 now prevent the Council from seeking pooled Section 106 financial contributions, the Council has used the alternative mechanism of using a condition to require a scheme to be put in place to mitigate impacts on the SPA. This is generally achieved through a planning obligation completed under section 111 of the Local Government Act (LGA). However, the Inspector noted that Planning Practice Guidance discourages the use of negatively worded conditions, and considered that this approach is not a robust one. In order to ensure that this situation does not arise again, the applicant has been asked to consider preparing a section 111 LGA obligation ahead of the decisions being made for both applications. Progress on this approach will be given in an update report.
- 6.16 The Council's ecologist considers that Natural England should be consulted for the applications due to the site's proximity to the Thames Basin Heaths SPA. However, provided that the applicant complies with the Council's agreed approach on mitigation of impacts on the SPA, as noted above, it is considered that Natural England no longer needs to comment individually on applications with respect to this issue.

Impacts on protected wildlife within the site

- 6.17 A survey has been undertaken at the site that has been reviewed by the Council's ecologist, who recommends appropriate conditions in the event that planning permission is granted.

For Application 2 only, the impacts of the roof terrace on neighbouring properties

- 6.18 The roof terrace for Flat 5 would be approximately 17m at its closest point from the closest windows at Richmond House, to the north-west, and from Hill House. As this terrace would be cut into the roof slope it appears that the roof itself would screen Richmond House from any direct views.
- 6.19 Views to habitable room windows at Hill House would be more direct; windows in the north-west elevation of Hill House directly face the roof terrace. While the distance between the two buildings may be sufficient to ensure that there would be no adverse impacts buildings, further clarification of this issue is required and it would be the recommendation at Section 1 takes this into account.

Other material considerations

- 6.20 The Highways Officer has not objected, but has requested a number of conditions in the event that planning permission is granted, including provision of visibility splays commensurate with the 40mph speed limit on Cross Road and gradient for the access ramp to the basement car parking.

Housing Land Supply

- 6.21 Paragraphs 7 and 14 of the National Planning Policy Framework (NPPF) set out that there will be a presumption in favour of Sustainable Development. Paragraph 49 of the NPPF states that applications for new homes should be considered in the context of the presumption in favour of sustainable development, and that relevant policies for the supply of housing should not be considered up-to-date if the local planning authority cannot demonstrate a five-year supply of deliverable housing sites. The Borough Council cannot demonstrate a 5 year housing land supply.
- 6.22 It is acknowledged that this scheme would make a contribution to the Borough's housing stock and it is the view of the Local Planning Authority that that the socio-economic benefits of the additional dwelling(s) would also weigh in favour of the development.

7. CONSULTATIONS CARRIED OUT

20 occupiers were notified directly of both applications.

The planning officer posted site notices for application ref. 16/00266/FULL at the site on 01.02.2016 and for application ref. 16/01179/FULL on 28.04.2016

Neighbour letters and consultation responses for the two applications are listed separately below:

Comments from interested parties for Application 1: 16/00266/FULL

Four letters were received objecting to the application, including one from an individual and one each from the Society for the Protection of Ascot and Environs and the Neighbourhood Plan Delivery Group. These are summarised as:

Comment	Where in the report this is considered
1. Impacts on privacy at Hillside Park.	6.5.
2. Impact on protected oak tree.	6.6 - 6.13.
3. Density of the development.	6.4.
4. Previous appeals for similar proposals have been dismissed.	6.2 - 6.13, 6.15.

Consultees' responses for Application 1: 16/00266/FULL

Consultee	Comment	Where in the report this is considered
Parish Council:	The revised application continues to be out of character for the area and the Inspector's main issues identified in the Appeal Decision rejections have not been addressed.	6.2 - 6.13.
	Seek clarification that the gradient/angle of the short driveway to access the basement parking area is compliant as the building line is now further forward.	6.20.
Highways Officer:	No objection; conditions requested in the event that planning permission is granted.	6.20.
Trees Officer:	No objection subject to conditions.	6.6 - 6.12.
Ecologist:	No objection on ground of impacts on protected wildlife within the site, subject to conditions. Considers that Natural England should be consulted for this application.	6.16, 6.17.

Comments from interested parties for Application 2: 16/01179/FULL

Six letters were received objecting to the application, including one from an individual and one each from the Society for the Protection of Ascot and Environs and the Neighbourhood Plan Delivery Group. These are summarised as:

Comment	Where in the report this is considered
1. Overdevelopment of the site and adverse impact on the character of	6.2 - 6.15.

	the area including increase in density.	
2.	Adverse impact on residential amenity and privacy.	6.5.
3.	Impact on daylight to neighbouring apartments.	6.5.
4.	Adverse impact on the protected oak tree.	6.6 - 6.13.
5.	Removal of other trees.	6.13.
6.	Impacts on protected wildlife.	6.17.
7.	Highways safety including difficulty of access to the A30.	6.20.
8.	The proposal does not overcome the Inspector's reasons for refusal.	6.6 - 6.13, 6.15.
9.	Poor level of amenity for future occupiers of Flat 5 due to their being no south-facing windows.	6.5.

Consultees' responses for Application 2: 16/01179/FULL

Consultee	Comment	Where in the report this is considered
Parish Council:	Concerns with impact on the protected oak tree noted in the appeal decision, and that the number of windows facing the tree is likely to lead to its future pruning.	6.2 - 6.13.
	Seek clarification that the gradient/angle of the short driveway to access the basement parking area is compliant as the building line is now further forward.	6.20.
Highways Officer:	No objection; conditions requested in the event that planning permission is granted.	6.20.
Trees Officer:	Objection.	6.6 - 6.12.
Ecologist:	No objection on ground of impacts on protected wildlife within the site, subject to conditions. Considers that Natural England should be consulted for this application.	6.16, 6.17.
Thames Water:	No objection.	Noted.

8. APPENDICES TO THIS REPORT

- Appendix A - site location plan
- Appendix B - site layout, elevation drawings and floor plans for application ref. 16/00266/FULL (Application 1)
- Appendix C - site layout, elevation drawings and floor plans for application ref. 16/01179/FULL (Application 2)
- Appendix D - elevation drawings and floor plans for extant permission 13/01206/FULL
- Appendix E - elevation drawings and floor plans for dismissed appeal ref. 14/03591/FULL
- Appendix F - elevation drawings and floor plans for dismissed appeal ref. 15/01199/FULL
- Appendix G - appeal decision for previous applications ref. 14/03591/FULL and 15/01199/FULL

This recommendation is made following careful consideration of all the issues raised through the application process and thorough discussion with the applicants. The Case Officer has sought solutions to these issues where possible to secure a development that improves the economic, social and environmental conditions of the area, in accordance with NPFF.

In this case the issues are considered able to be successfully resolved.

9. CONDITIONS RECOMMENDED FOR INCLUSION IF PERMISSION IS GRANTED

1. The development hereby permitted shall be commenced within three years from the date of this permission.
Reason: To accord with the provisions of Section 91 of the Town and Country Planning Act 1990 (as amended).
2. No site clearance or excavation shall commence in association with the development until a biodiversity mitigation strategy, including details of provision of nesting boxes and other habitat provision / improvements (which should be incorporated into the landscaping proposals for the site), has been submitted to and approved in writing by the Local Planning Authority. The approved mitigation measures shall then be implemented in their entirety within the timescales approved within the strategy.
Reason: In order to comply with Neighbourhood Plan Policy NP/E4 and with advice in the National Planning Policy Framework 2012.
3. Prior to any equipment, machinery or materials being brought onto the site and prior to any demolition works in connection with the development, details of the measures to protect, during construction and demolition, the trees to be retained within the development shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be implemented in full prior to any demolition works or before any equipment, machinery or materials are brought onto the site, and shall then be maintained until the completion of all construction work and all equipment, machinery and surplus materials have been permanently removed from the site. These measures shall include fencing in accordance with British Standard 5837:2012. Nothing shall be stored or placed in any area fenced in accordance with this condition and the ground levels within those areas shall not be altered, nor shall any excavation be made, without the prior written approval of the Local Planning Authority.
Reason: To protect trees which contribute to the visual amenities of the site and surrounding area. Relevant Policies - Local Plan DG1 and N6.
4. Prior to the commencement of any works of demolition or construction a management plan showing how demolition and construction traffic, (including cranes), materials storage, facilities for operatives and vehicle parking and manoeuvring will be accommodated during the works period shall be submitted to and approved in writing by the Local Planning Authority. The plan shall be implemented as approved and maintained for the duration of the works or as may be agreed in writing by the Local Planning Authority.
Reason: In the interests of highway safety and the free flow of traffic. Relevant Policies - Local Plan T5.
5. No development shall take place until detailed drawings of the access have been submitted to and approved in writing by the Local Planning Authority, including details that demonstrate that the ramp to the basement parking does not exceed 1:12 (with adequate transitions) to ensure that safe and satisfactory access can be provided and assist with refuse / cycle access. The access shall then be constructed in accordance with the approved details and retained as such.
Reason: In the interests of highway safety and the free flow of traffic. Relevant Policies - Local Plan T5 and DG1.
6. No development shall take place until samples of the materials to be used on the external surfaces of the development have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out and maintained in accordance with the approved details.
Reason: In the interests of the visual amenities of the area. Relevant Policy DG1
7. No development shall take place until details of measures to ensure that the proposed apartments are suitable for occupation by people of all age groups, including the elderly, have been submitted to and approved in writing by the Local Planning Authority. These details shall demonstrate how the development would meet the standards, including the Lifetime Homes

standard, as set out in the Royal Borough of Windsor and Maidenhead Planning for an Ageing Population Supplementary Planning Document. The development shall be carried out and subsequently retained and maintained in accordance with the approved details.

Reason: To ensure that measures to make the development accessible for all age groups are included in the development and to comply with the Royal Borough of Windsor and Maidenhead Planning for an Ageing Population Supplementary Planning Document.

8. No development shall take place until full details of both hard and soft landscape works, including boundary treatment, have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved within the first planting season following the substantial completion of the development and retained in accordance with the approved details. Details to be included in the submission shall include plant numbers, grades and densities, and materials to be used in hard surfaced areas and any fences or walls. If within a period of five years from the date of planting of any tree or shrub shown on the approved landscaping plan, that tree or shrub, or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted in the immediate vicinity, unless the Local Planning Authority gives its prior written consent to any variation.
Reason: To ensure a form of development that maintains, and contributes positively to, the character and appearance of the area. Relevant Policy - Local Plan DG1.
9. Prior to the commencement of development, a plan showing the position of all underground services in relation to the root protection areas of retained trees and hedges and proposed soft landscaping shall be submitted to and approved in writing by the Local Planning Authority. All underground services shall then be provided only in accordance with the approved details and maintained as such, unless otherwise first approved in writing by the Local Planning Authority.
Reason: To protect trees which contribute to the visual amenities of the site and surrounding area. Relevant Policies - Local Plan DG1 and N6.
10. No other part of the development shall commence until the access has been constructed in accordance with the approved drawing. The access shall thereafter be retained.
Reason: In the interests of highway safety and the free flow of traffic. Relevant Policies - Local Plan T5, DG1.
11. No development shall commence until details of all finished slab and roof levels in relation to ground level (against OD Newlyn and including roof levels for Richmond House and other buildings close to the application site) have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out and maintained in accordance with the approved details.
Reason: In the interest of the visual amenities of the area. Relevant Policy Local Plan DG1.
12. No part of the development shall be occupied until vehicle parking space has been provided in accordance with the approved drawing. The space approved shall be retained for parking in association with the development.
Reason: To ensure that the development is provided with adequate parking facilities in order to reduce the likelihood of roadside parking which could be detrimental to the free flow of traffic and to highway safety. Relevant Policies - Local Plan P4, DG1.
13. No part of the development shall be commenced until visibility splays of 2.4m metres by 43m metres have been provided at the site entrance. All dimensions are to be measured along the edge of the driveway and the back of footway from their point of intersection. The areas within these splays shall be kept free of all obstructions to visibility over a height of 0.6 metres above carriageway level.
Reason: In the interests of highway safety. Relevant Policies - Local Plan T5.
14. No part of the development shall be occupied until a refuse bin storage area and recycling facilities have been provided in accordance with details that have first been submitted to and approved in writing by the Local Planning Authority. These facilities shall be kept available for use in association with the development at all times.

Reason: To ensure that the development is provided with adequate facilities that allow it to be serviced in a manner which would not adversely affect the free flow of traffic and highway safety and to ensure the sustainability of the development. Relevant Policies - Local Plan T5, DG1.

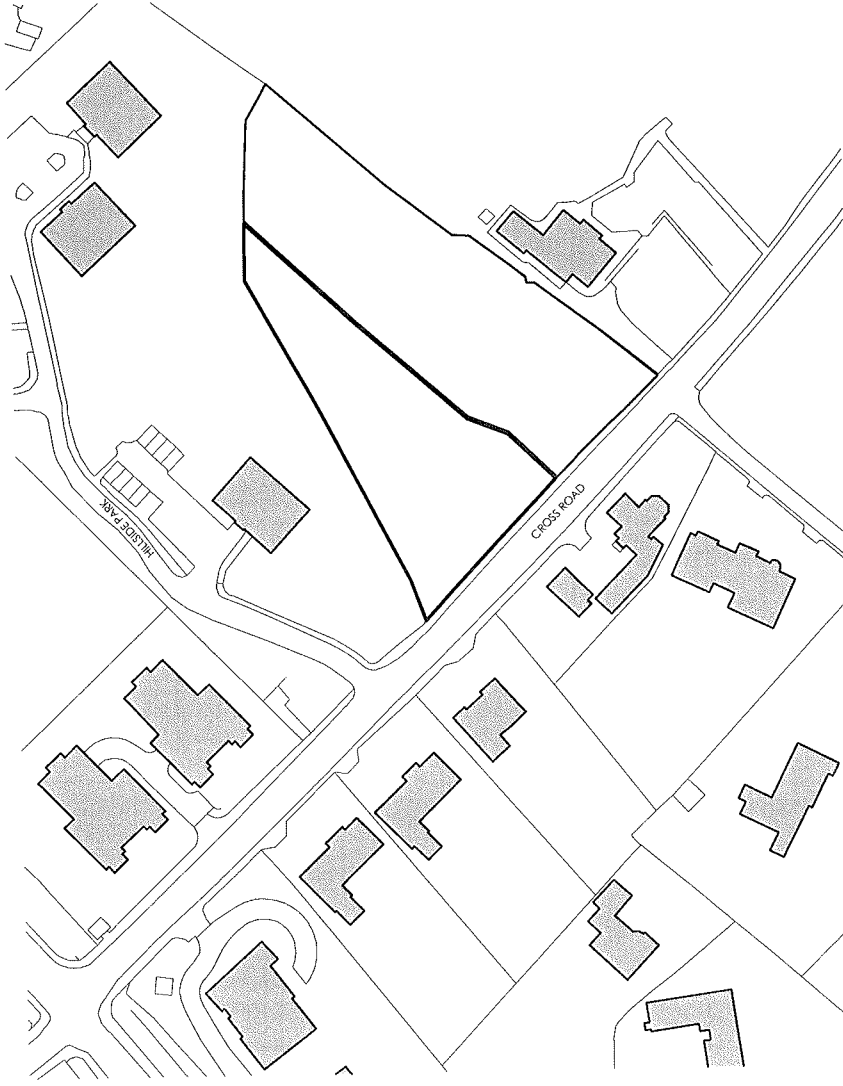
15. No part of the development shall be occupied until covered and secure cycle parking facilities have been provided in accordance with the approved drawing. These facilities shall thereafter be kept available for the parking of cycles in association with the development at all times.
Reason: To ensure that the development is provided with adequate cycle parking facilities in order to encourage the use of alternative modes of transport. Relevant Policies - Local Plan T7, DG1.
16. The hard surface shall be made of porous materials and retained thereafter or provision shall be made and retained thereafter to direct run-off water from the hard surface to a permeable or porous area or surface within the curtilage of the property.
Reason: To reduce the risk of flooding and pollution and increase the level of sustainability of the development and to comply with Requirement 5 of the Royal Borough of Windsor & Maidenhead Sustainable Design & Construction Supplementary Planning Document.
17. No outdoor lighting may be provided at the site unless details have first been submitted to and agreed in writing by the Local Planning Authority. The development shall then be implemented and maintained in accordance with the approved details.
Reason: To provide a development that is complementary to this edge of settlement location. Relevant Policy - Local Plan DG1.
18. The development hereby permitted shall be carried out in accordance with the approved plans listed below.
Reason: To ensure that the development is carried out in accordance with the approved particulars and plans.

Informatives

1. The Streetcare Services Manager at Tinkers Lane Depot Tinkers Lane Windsor SL4 4LR tel: 01628 796801 should be contacted for the approval of the access construction details and to grant a licence before any work is carried out within the highway. A formal application should be made allowing at least 4 weeks notice to obtain details of underground services on the applicant's behalf.
2. The attention of the applicant is drawn to the Berkshire Act 1986, Part II, Clause 9, which enables the Highway Authority to recover the costs of repairing damage to the footway or grass verge arising during building operations.
3. The attention of the applicant is drawn to Section 59 of the Highways Act 1980 which enables the Highway Authority to recover expenses due to extraordinary traffic.
4. The granting of planning permission does not give the applicant/developer consent to carry out works on or affecting the public highway (verge, footway or carriageway). To obtain consent from the Highway Authority, not less than 4 weeks' notice shall be given to the Council's Streetcare Section (tel: 01628 683804) before any work is carried out. This shall include for material and skips which are stored within the public highway extents, hoarding etc. A charge will be made for the carrying out of inspections and the issue of permits.

APPENDIX A.

Rev.	Description	Date



Status: PLANNING

Scale: 1 : 1250 Date: Oct 2014

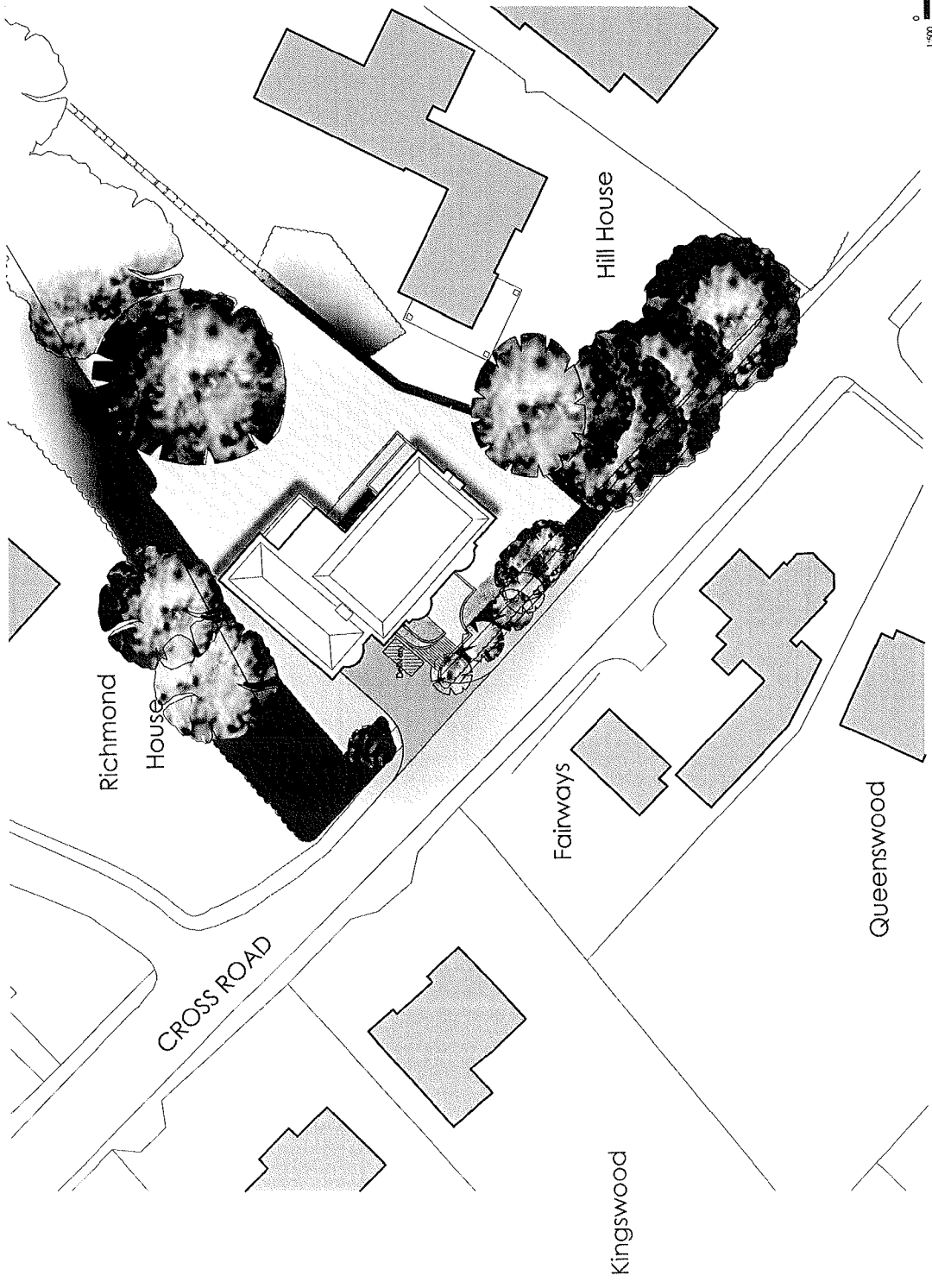
Revision: Sheet: A3

Dwg No: FD 14 - 1220 P2 - 50

Planning
This drawing is produced for PLANNING PURPOSES ONLY and is not fit for use for construction

Client: KEBBELL DEVELOPMENT LTD.
Project: Land at Hill House, Cross Road, Sunningdale
Title: Site Location Plan

Rev	Description	Date



Status: PLANNING

Scale: 1 : 500 Date: Oct 2014

Revision: A3 Sheet: A3

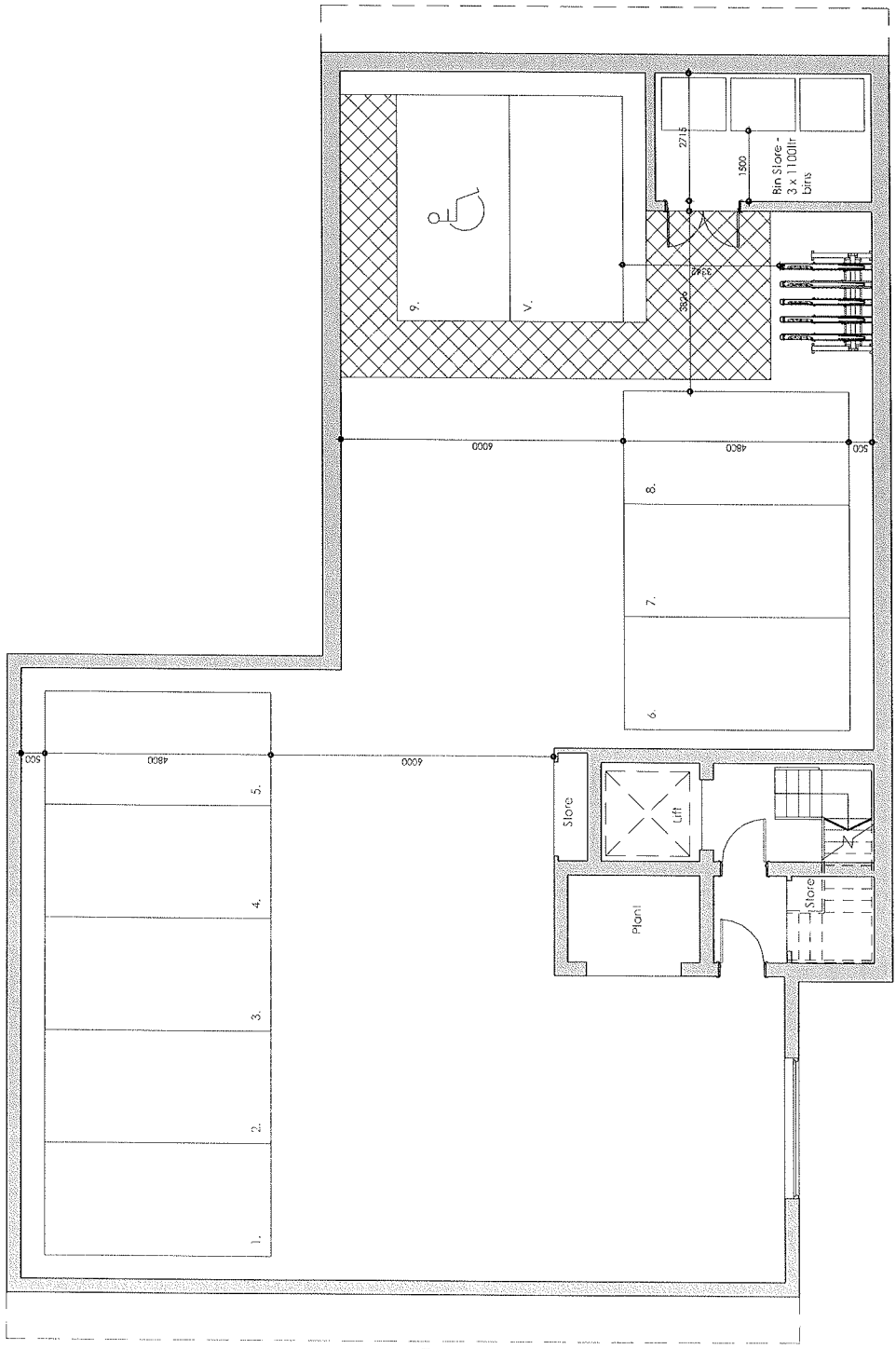
Drng No: FD 14 - 1220 P2 - 55

Planning
This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

Application 1: 16/00266/FULL

Client: KEBBELL DEVELOPMENT LTD.
Project: Land at Hill House, Cross Road, Sunningdale
Title: Proposed Site Layout

Rev	Description	Date



BASEMENT PLAN



Status: PLANNING
Scale: 1 : 100
Revision: A3
Drwg No: FD 14 - 1220 P2 - 100
Date: Oct 2014
Sheet: A3
Planning Permission is required for PLANNING PURPOSES ONLY. Plans not to be used for CONSTRUCTION.

Application 1: 16/00266/FULL

Client: KEBBELL DEVELOPMENT LTD.
Project: Land at Hill House, Cross Road, Sunningdale
Title: Basement Plan

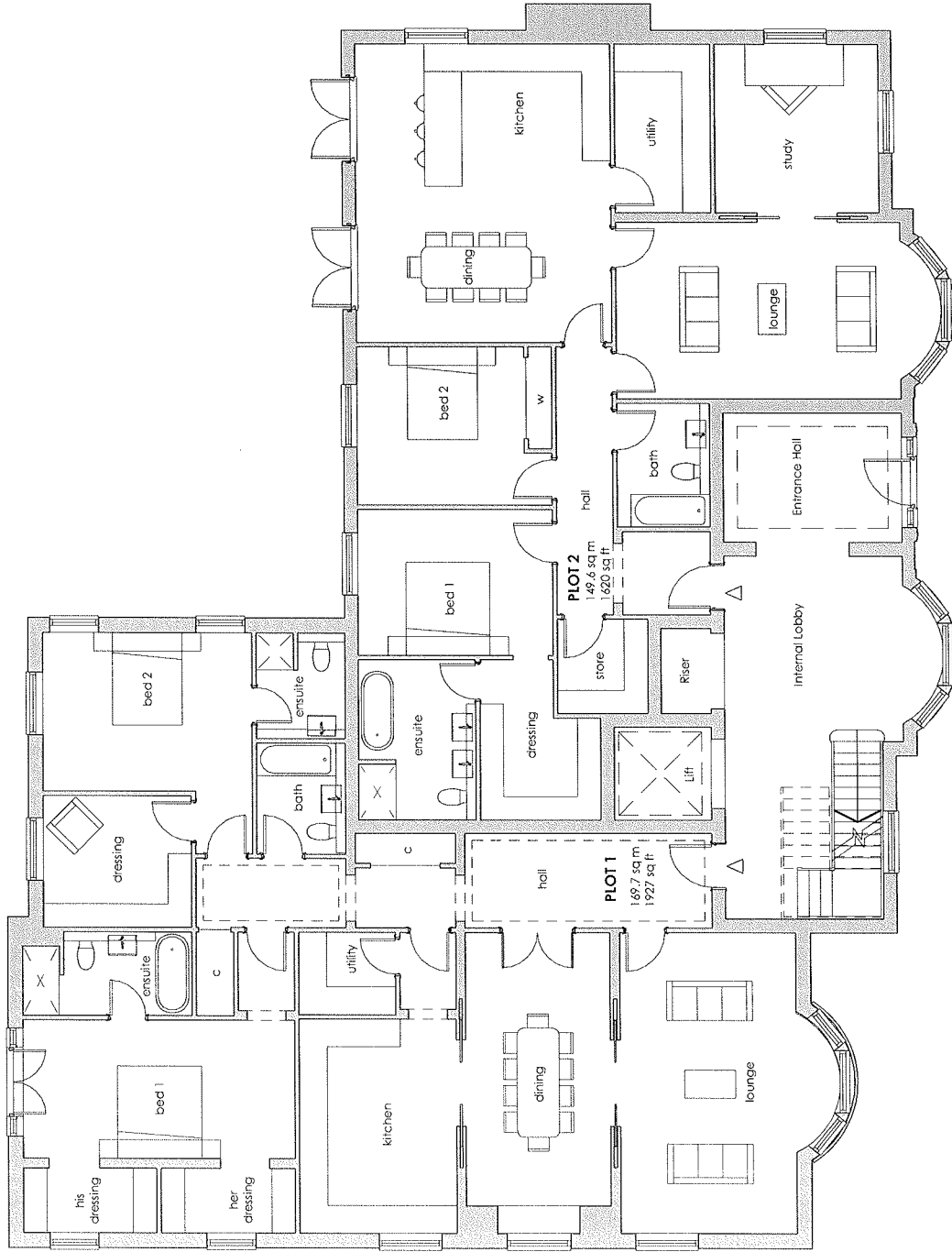
Ventilation to be provided along this wall

Ventilation to be located along this wall

Falco stacking bike rack
10 x lockable bike spaces

Notes:

Rev.	Description	Date



LOWER GROUND FLOOR PLAN



Status: PLANNING

Scale: 1 : 100

Date: Oct 2014

Revision: A3

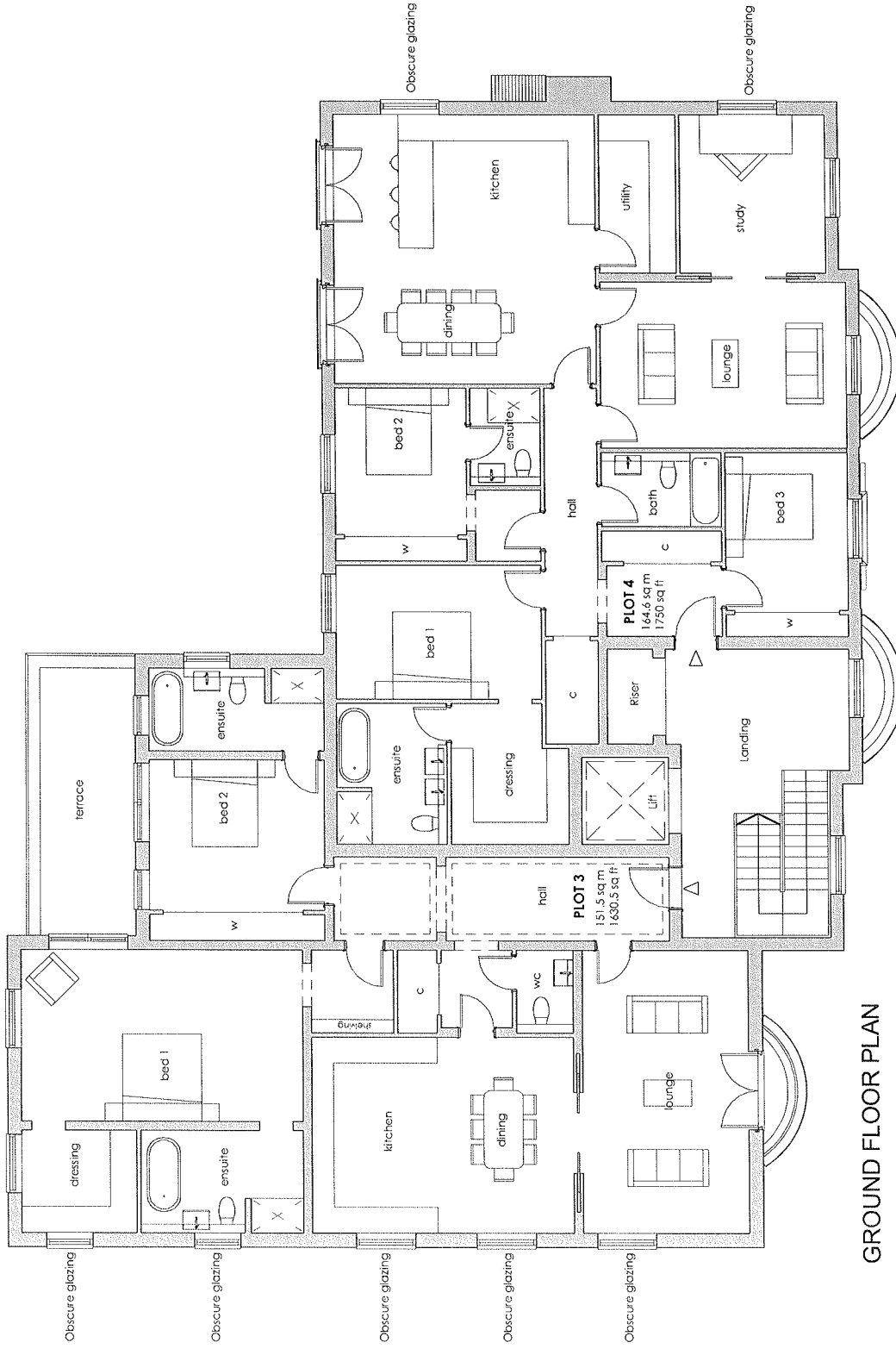
Sheet: A3

Drwg No: FD 14 - 1220 P2 - 101

Planning
This drawing is produced for PLANNING PURPOSES ONLY and is not for use in construction.

Application 1: 16/00266/FULL

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Lower Ground Floor Plan



GROUND FLOOR PLAN

Status: PLANNING

Scale: 1 : 100

Date: Oct 2014

Revision: A3

Sheet: A3

Drg No: FD 14-1220 P2-102

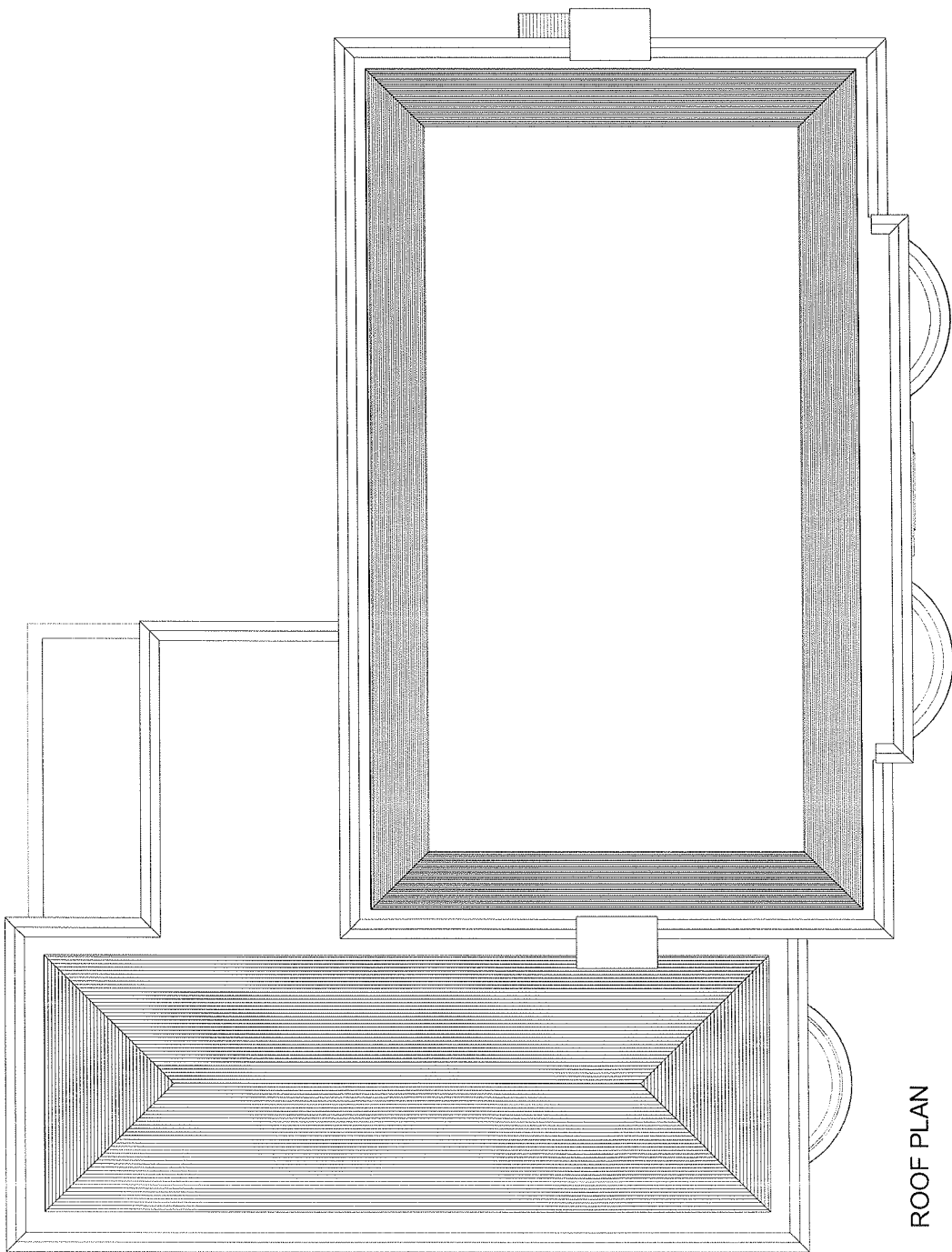
Planning permission for PLANNING PURPOSES ONLY. NOT VALID FOR CONSTRUCTION.

Appl. carbon 1: 16/00266/FULL

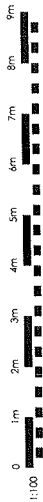
Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Ground Floor Plan

Rev / Description

Date



ROOF PLAN



Status: PLANNING

Scale: 1 : 100 Date: Oct 2014

Revision: A3 Sheet: A3

Drng No: FD 14 - 1220 P2 - 104

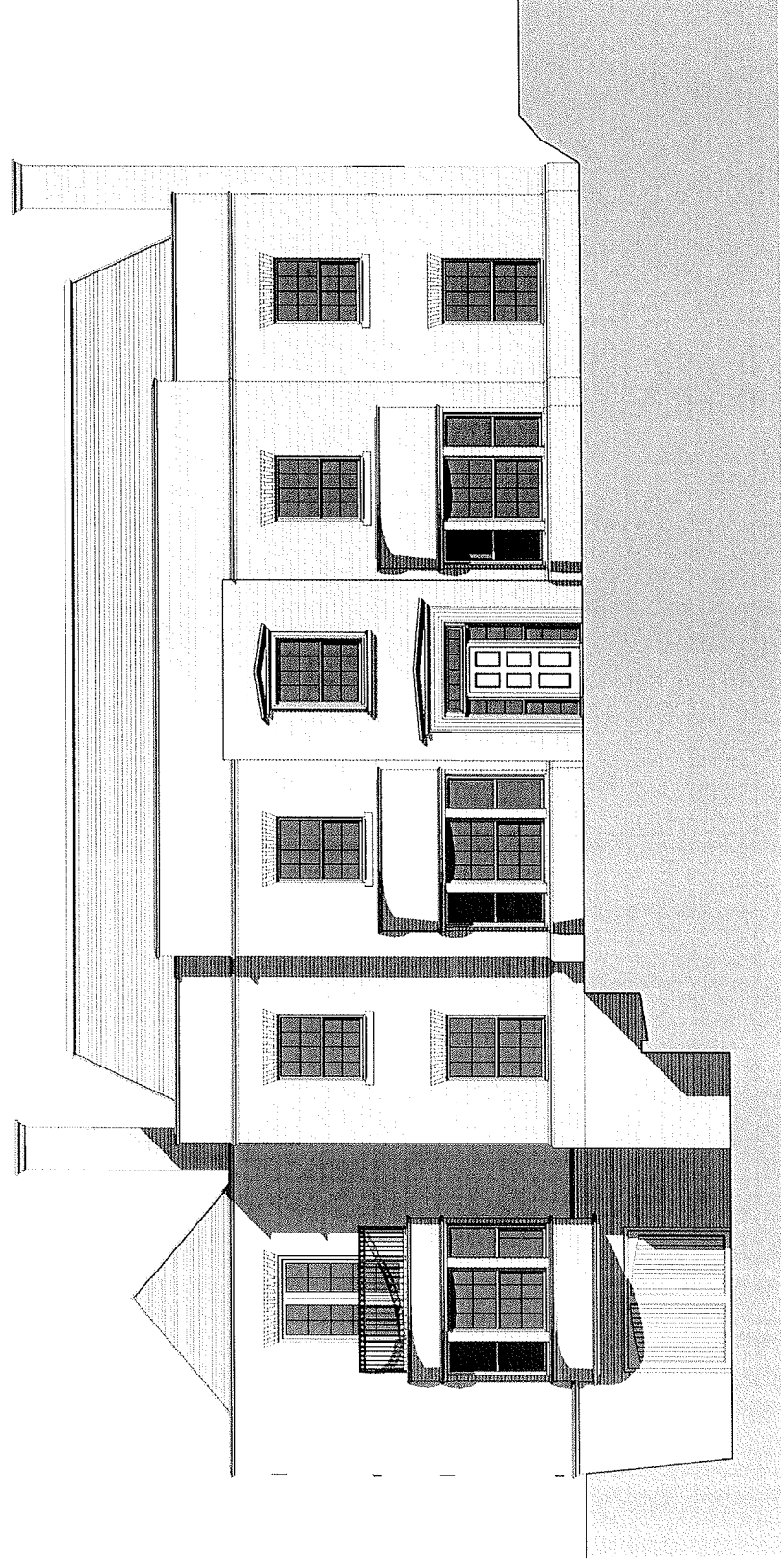
This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction

Application 1: 16/00266/FULL

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Roof Plan

Notes

Rev.	Description	Date



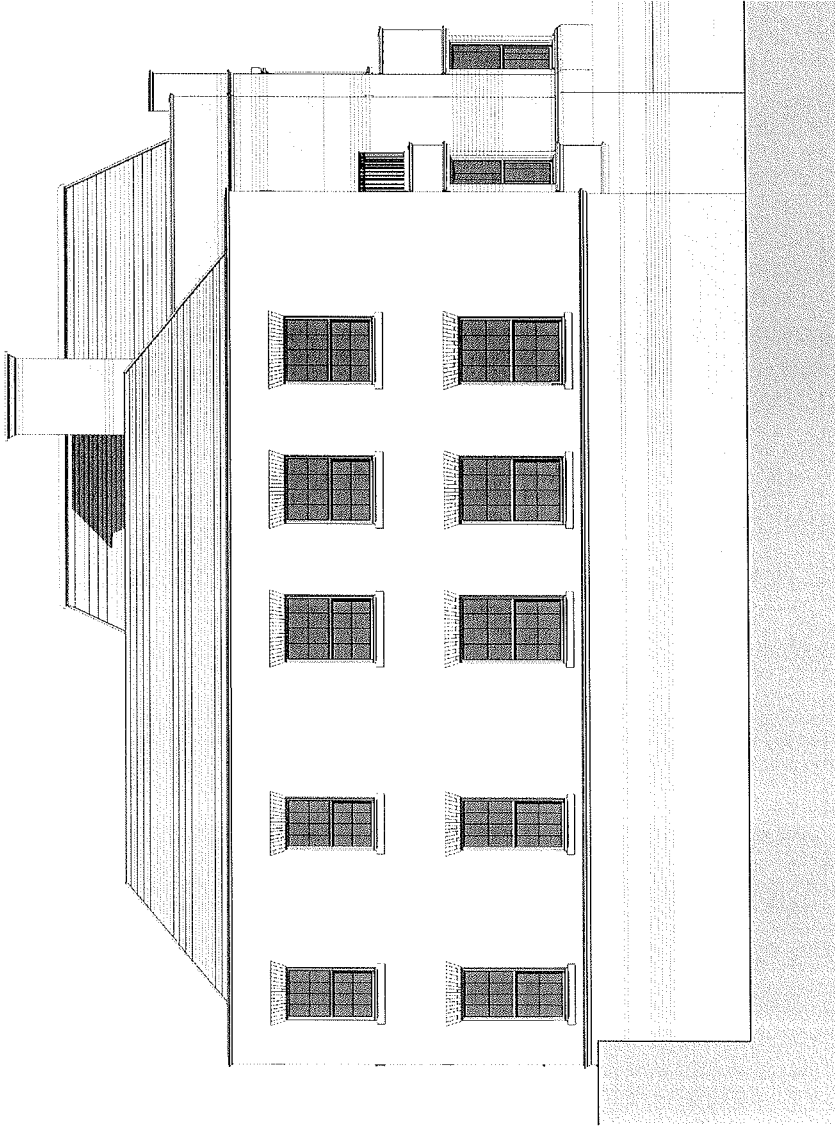
FRONT ELEVATION

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Front Elevation

Status: PLANNING
 Scale: 1 : 100 Date: Oct 2014
 Revision: A3 Sheet: A3
 Drg No: FD 14 - 1220 P2 - 110
 Drawing prepared for PLANNING PURPOSES ONLY and not to be used for CONSTRUCTION

Application 1: 16/00266/FULL

Rev. Description Date



LEFT FLANK ELEVATION



Status: PLANNING

Scale: 1 : 100

Date: Oct 2014

Revision: A3

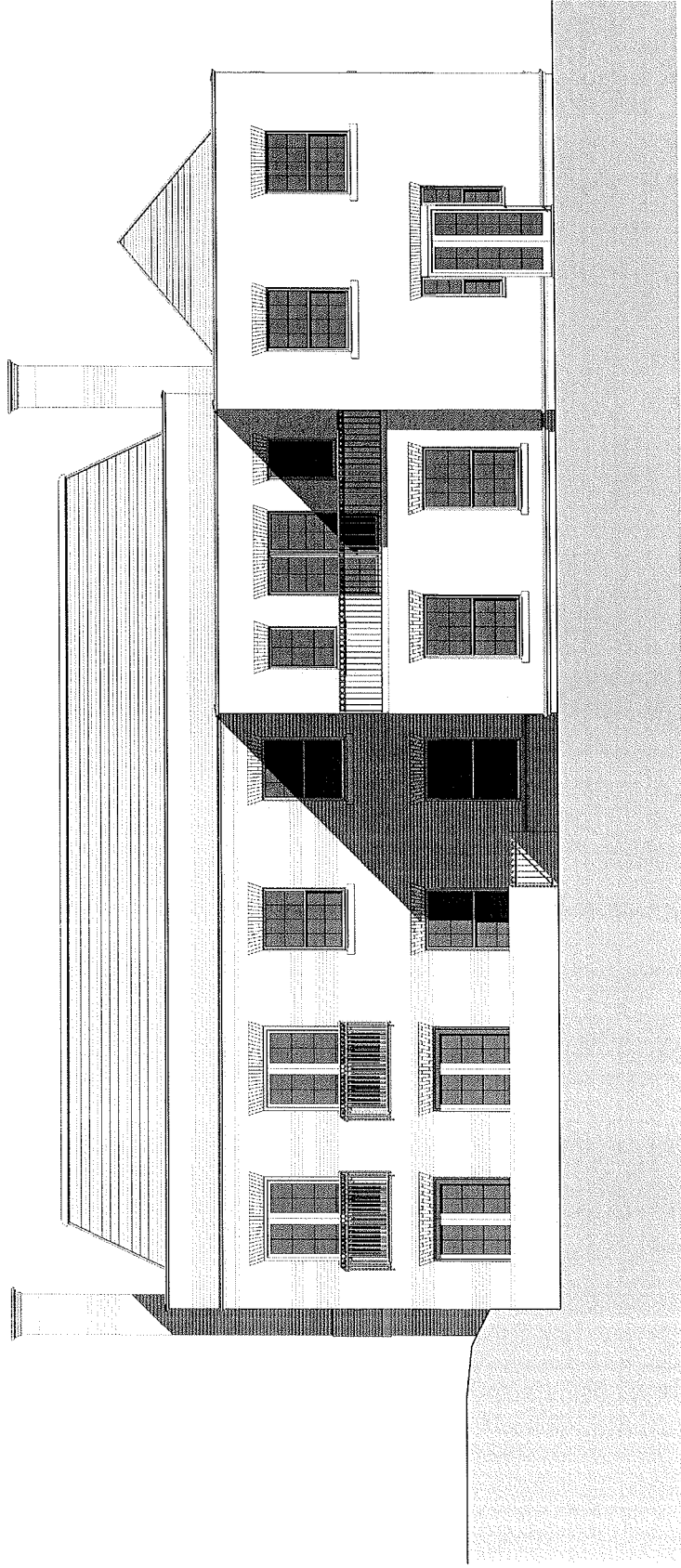
Drng No: FD 14-1220 P2-111

Planning
This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Left Flank Elevation

Application 1: 16/00266/FULL

Rev. Description Date



REAR ELEVATION



Status: PLANNING

Scale: 1 : 100

Date: Oct 2014

Revision: A3

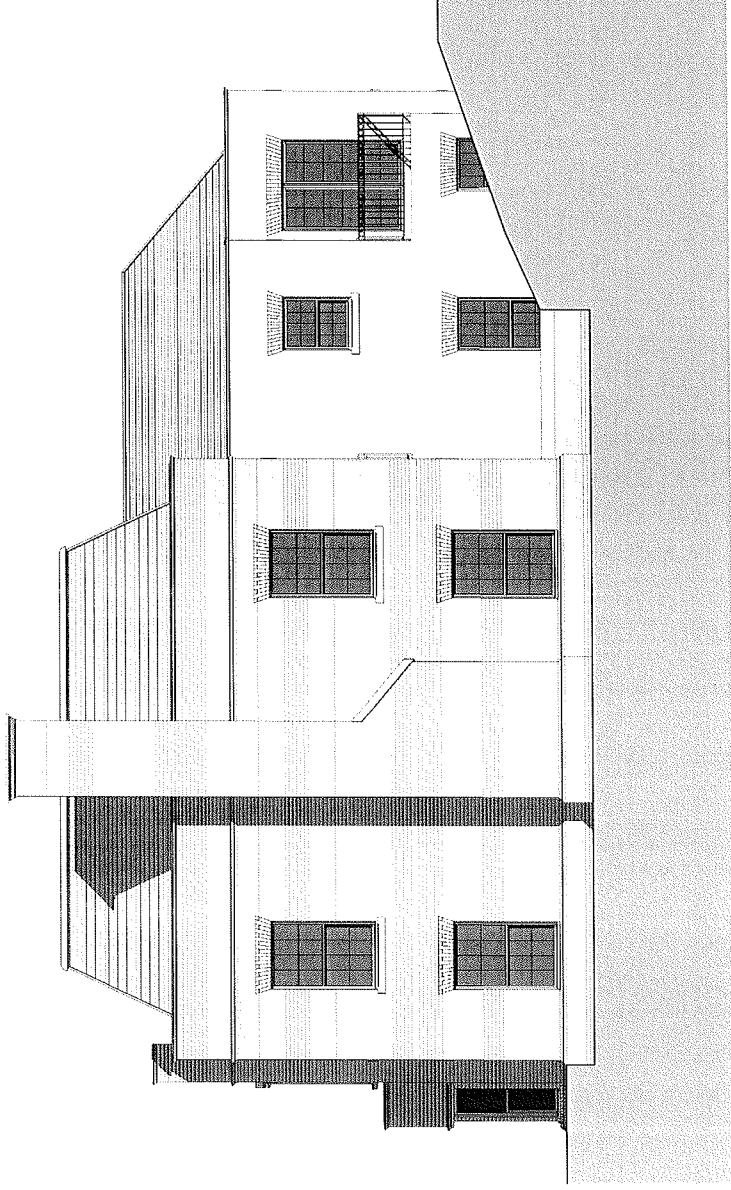
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Drwg No: FD 14-1220 P2-112

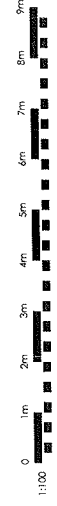
This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Rear Elevation

Application 1: 16/00266/FULL



RIGHT FLANK ELEVATION



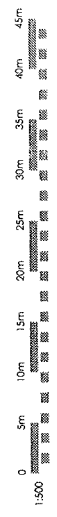
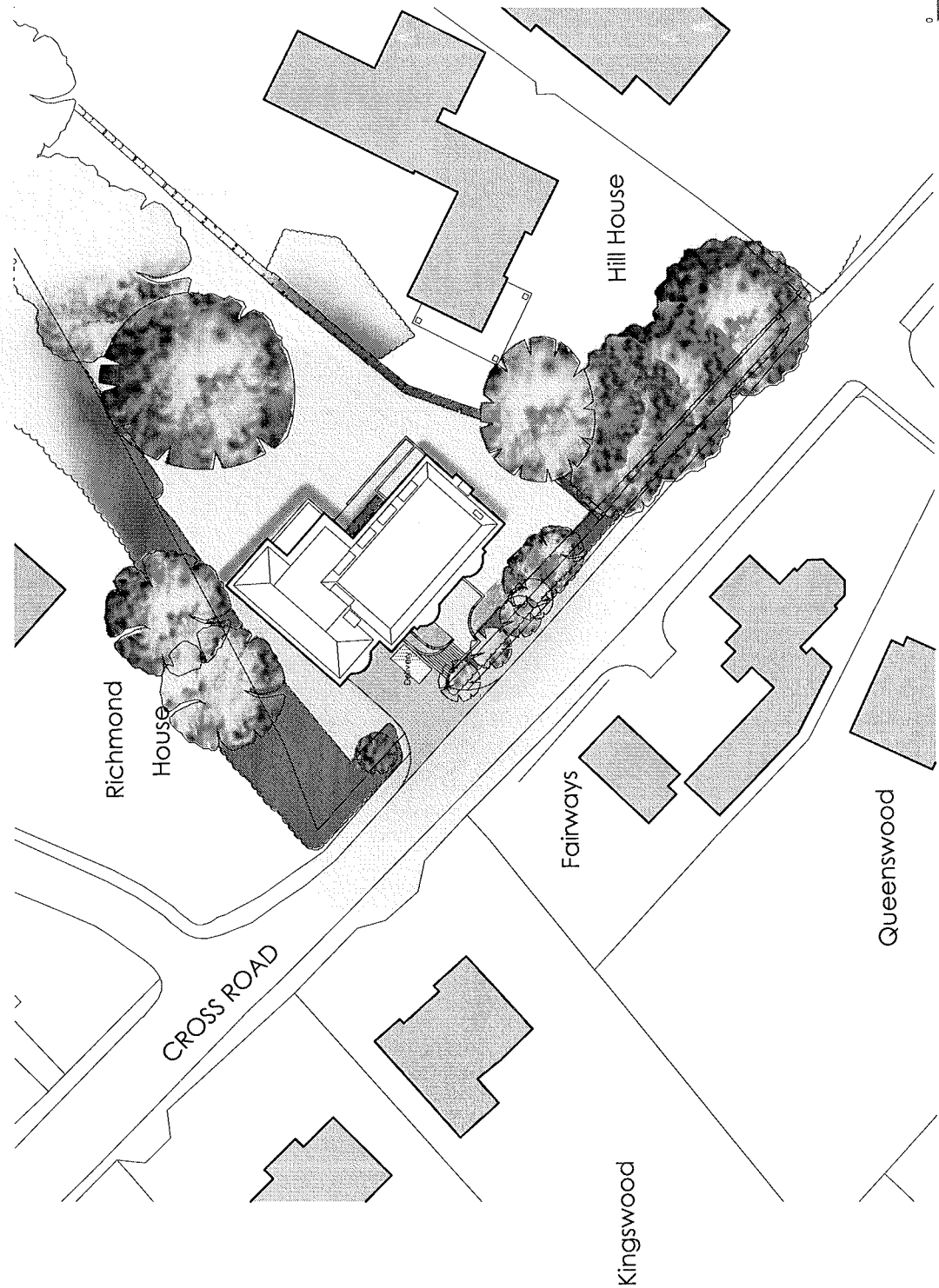
Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Summingdale
 Title: Right Flank Elevation

Application 1: 16/00266/FULL

Status: PLANNING
 Scale: 1 : 100 Date: Oct 2014
 Revision: A3
 Drg No: FD 14-1220 P2-113
 Planning
 This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

APPENDIX C

Rev.	Description	Date

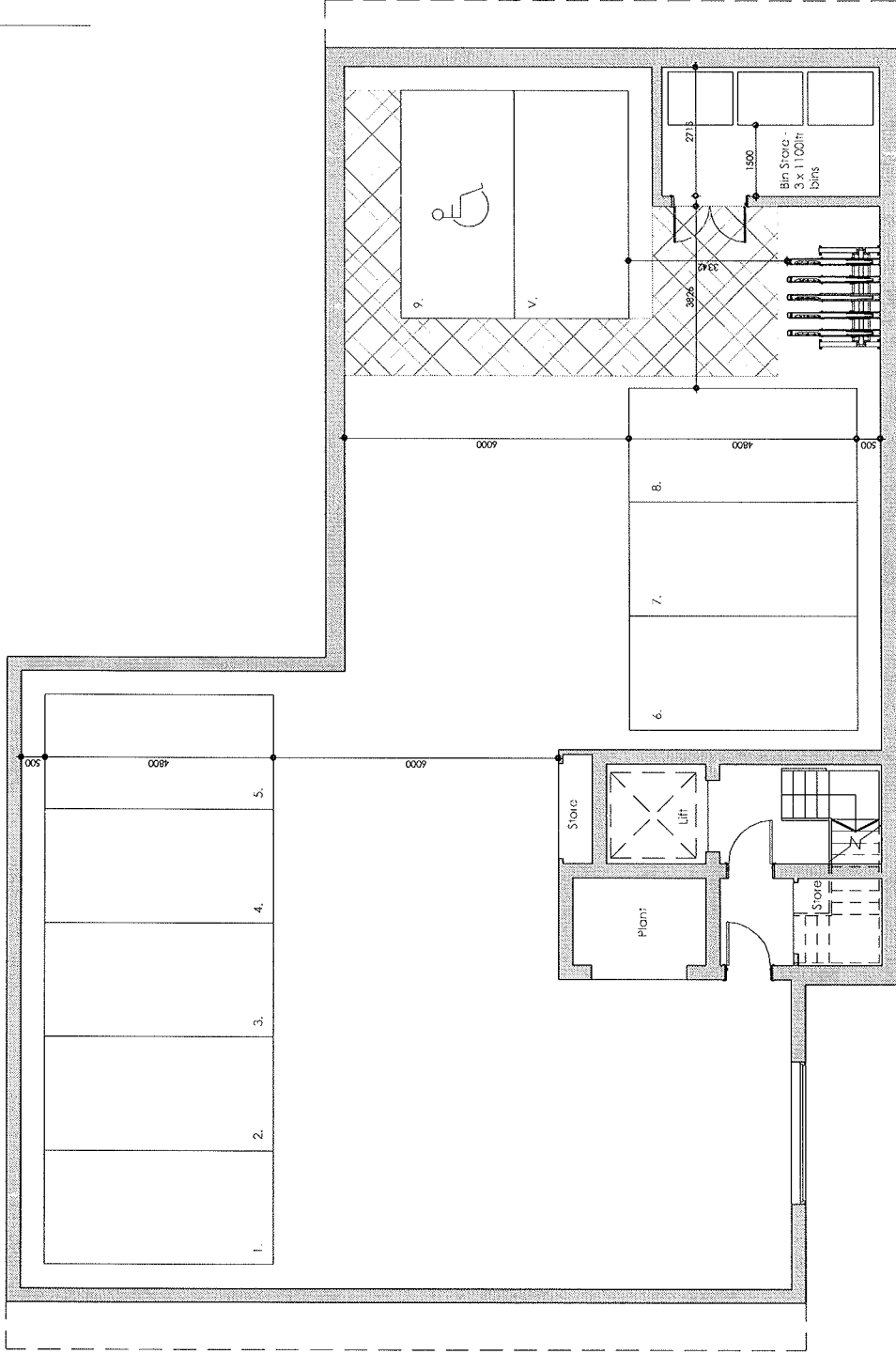


Status: PLANNING
Scale: 1 : 500
Revision: A3
Drwg No: FD 14-1220 P3-55
Date: April 2016
Sheet: A3
Planning
The drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

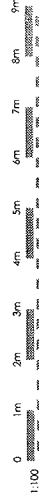
Application 2: 16/01179/FULL

Client: KEBBELL DEVELOPMENT LTD.
Project: Land at Hill House, Cross Road, Sunningdale
Title: Proposed Site Layout

Notes



BASEMENT PLAN



Status: **PLANNING**

Scale: 1 : 100 Date: April 2016

Revision: A3

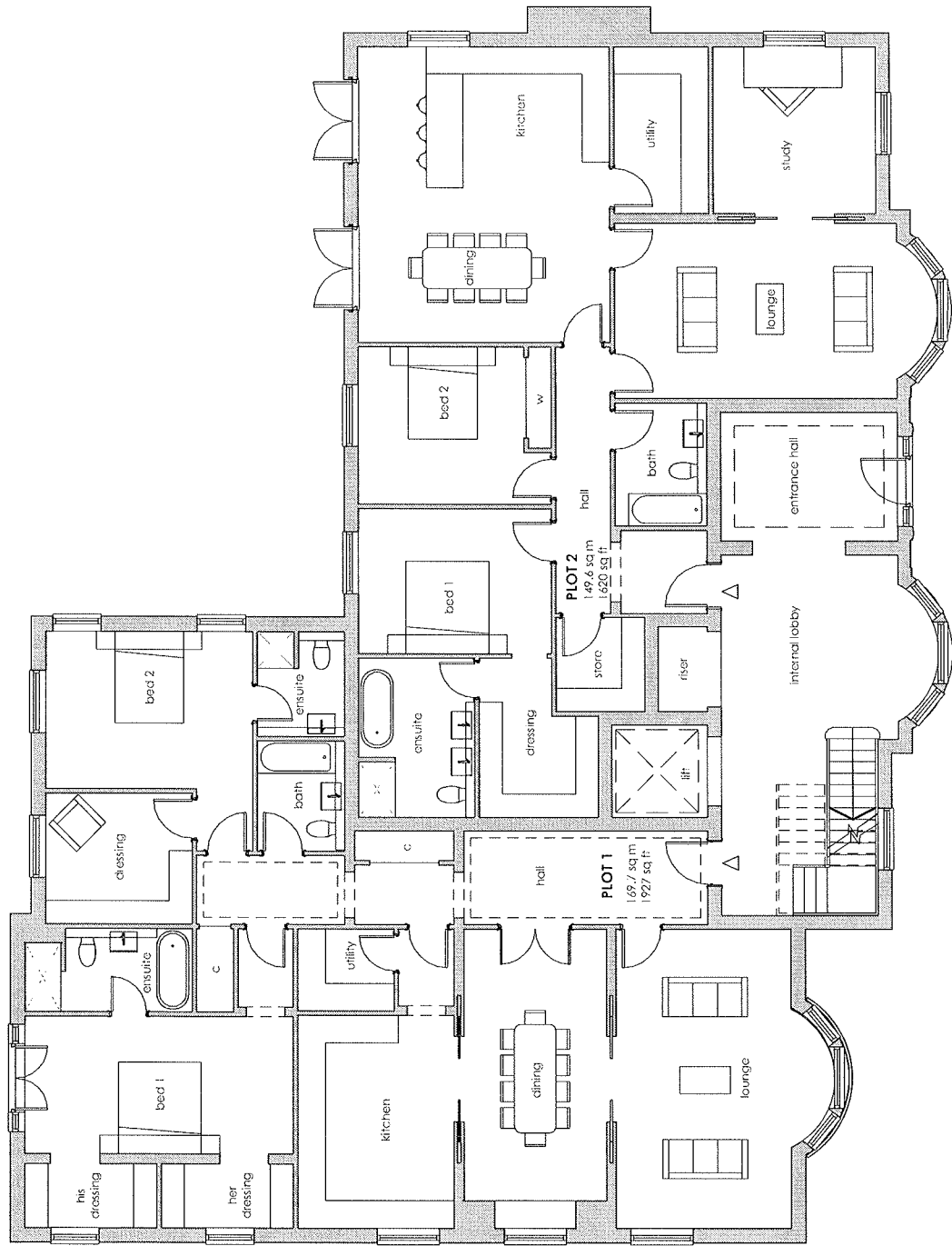
Drwg No: **FD 14 - 1220 P3 - 100**

Planning
The drawings are prepared for PLANNING PURPOSES ONLY and shall not be used for construction.

Application 2: 16/01/179/FULL

Client: **KEBBELL DEVELOPMENT LTD.**
 Project: **Land at Hill House, Cross Road, Sunningdale**
 Title: **Basement Plan**

Rev	Description	Date



LOWER GROUND FLOOR PLAN

Status: PLANNING

Scale: 1 : 100 Date: April 2016

Revision: A3

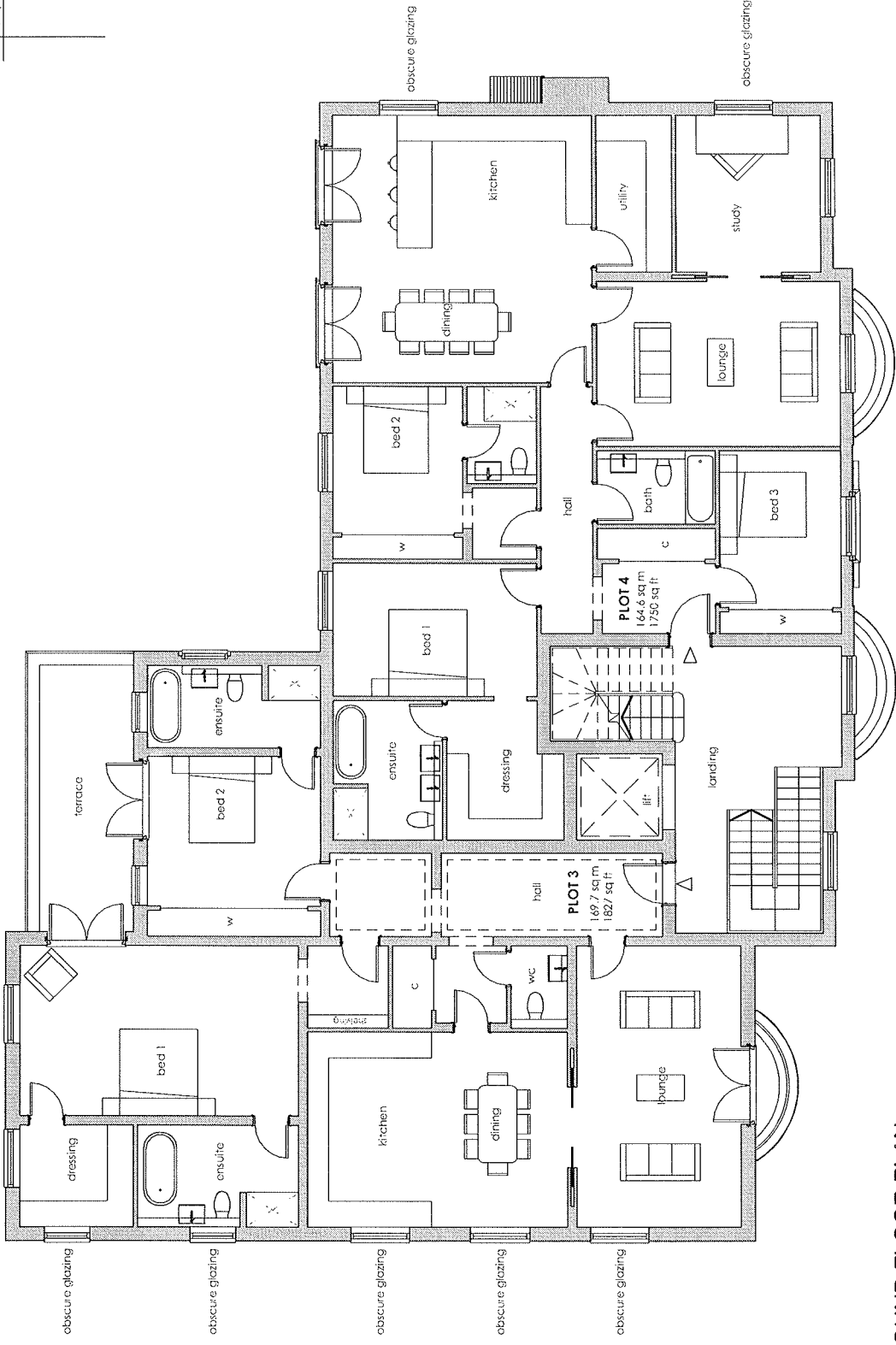
Dwg No: FD 14 - 1220 P3 - 101

Planning
This subject is prepared for PLANNING PURPOSE ONLY and is not to be used for construction.

Application 2: 16/01179/FULL

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Lower Ground Floor Plan

Rev	Description	Date



GROUND FLOOR PLAN

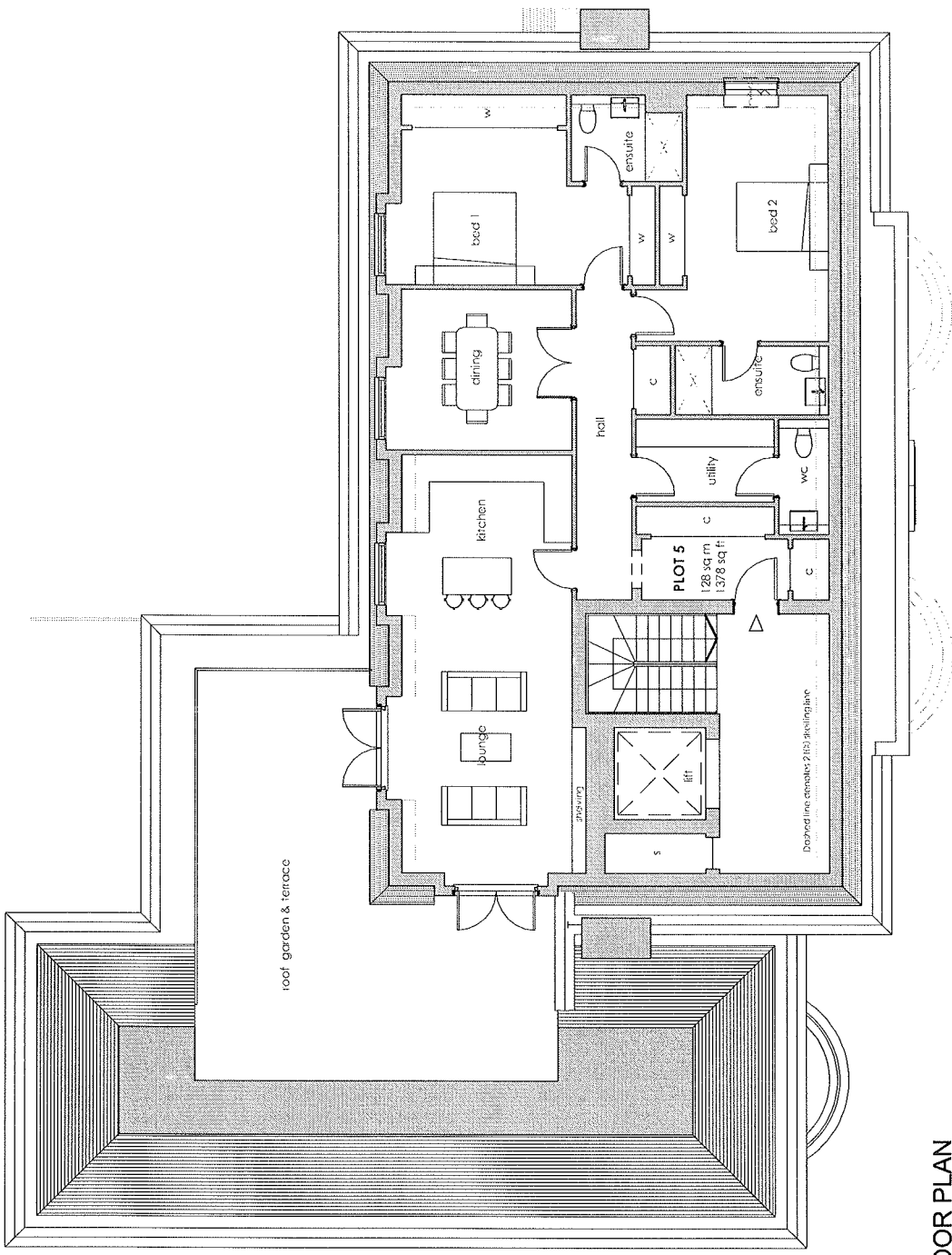


Status: PLANNING
Scale: 1 : 100
Date: April 2016
Revision: Sheet: A3
Drwg No: FD 14 - 1220 P3 - 102
Planning
This drawing is prepared for PLANNING PURPOSES ONLY and is not to be used for construction.

Application 2: 16/01179/FULL

Client: KEBBELL DEVELOPMENT LTD.
Project: Land at Hill House, Cross Road, Sunningdale
Title: Ground Floor Plan

Rev	Description	Date



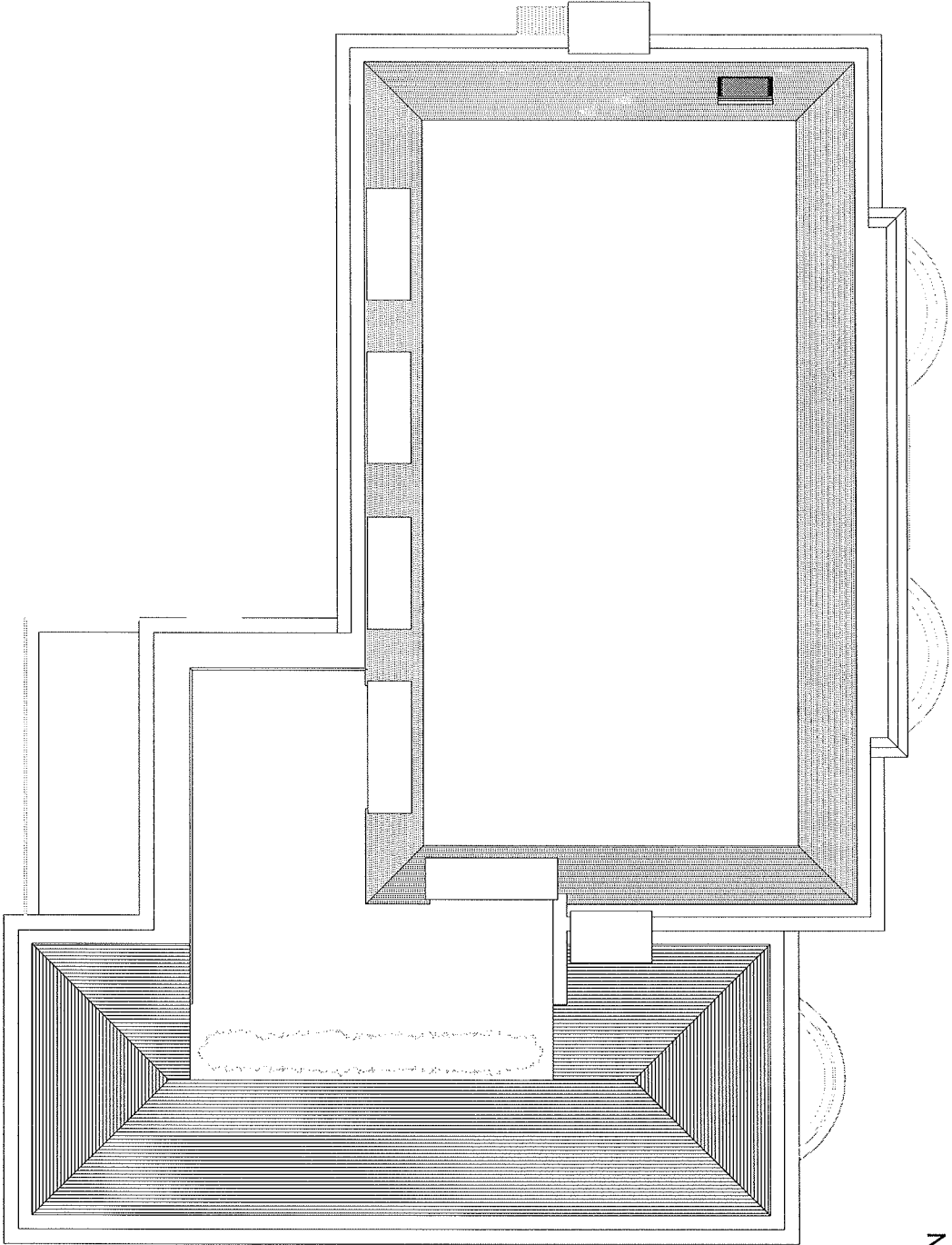
FIRST FLOOR PLAN

Application 2: 16/01179/FULL

Status: PLANNING
Scale: 1 : 100
Revision: A3
Drng No: FD 14 - 1220 P3 - 103
The drawings produced for PLANNING PURPOSES ONLY are not to be used for construction.

Client: KEBBELL DEVELOPMENT LTD.
Project: Land at Hill House, Cross Road, Sunningdale
Title: First Floor Plan

Rev.	Description	Date



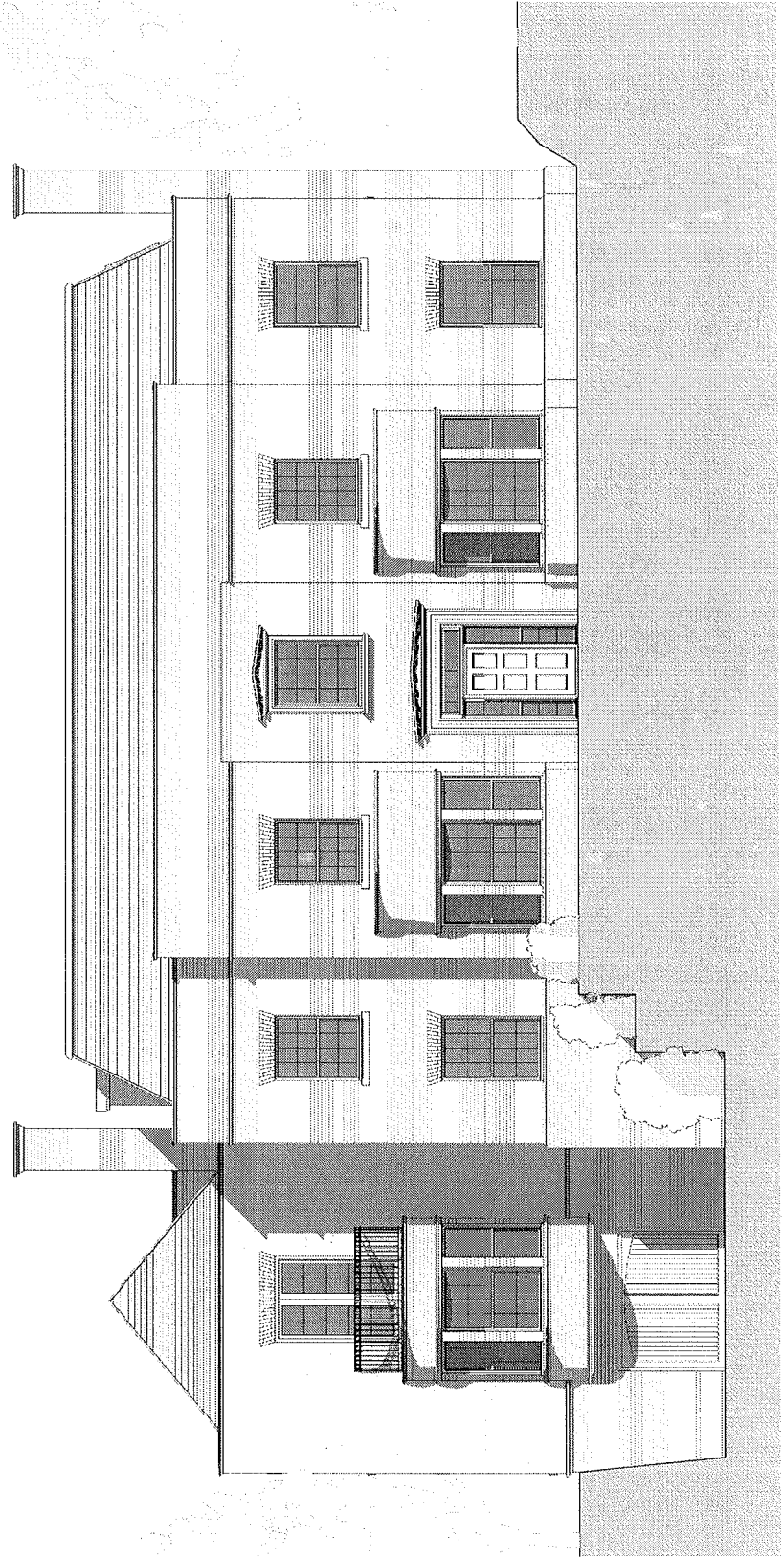
ROOF PLAN

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Roof Plan

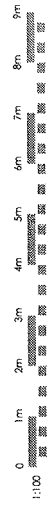
Application 2: 16/01179/FULL

Status: PLANNING
 Scale: 1 : 100
 Revision: A3
 Drg No: FD 14 - 1220 P3 - 104
 Drawing produced for PLANNING PURPOSES ONLY and not to be used for construction.

Rev	Description	Date



FRONT ELEVATION

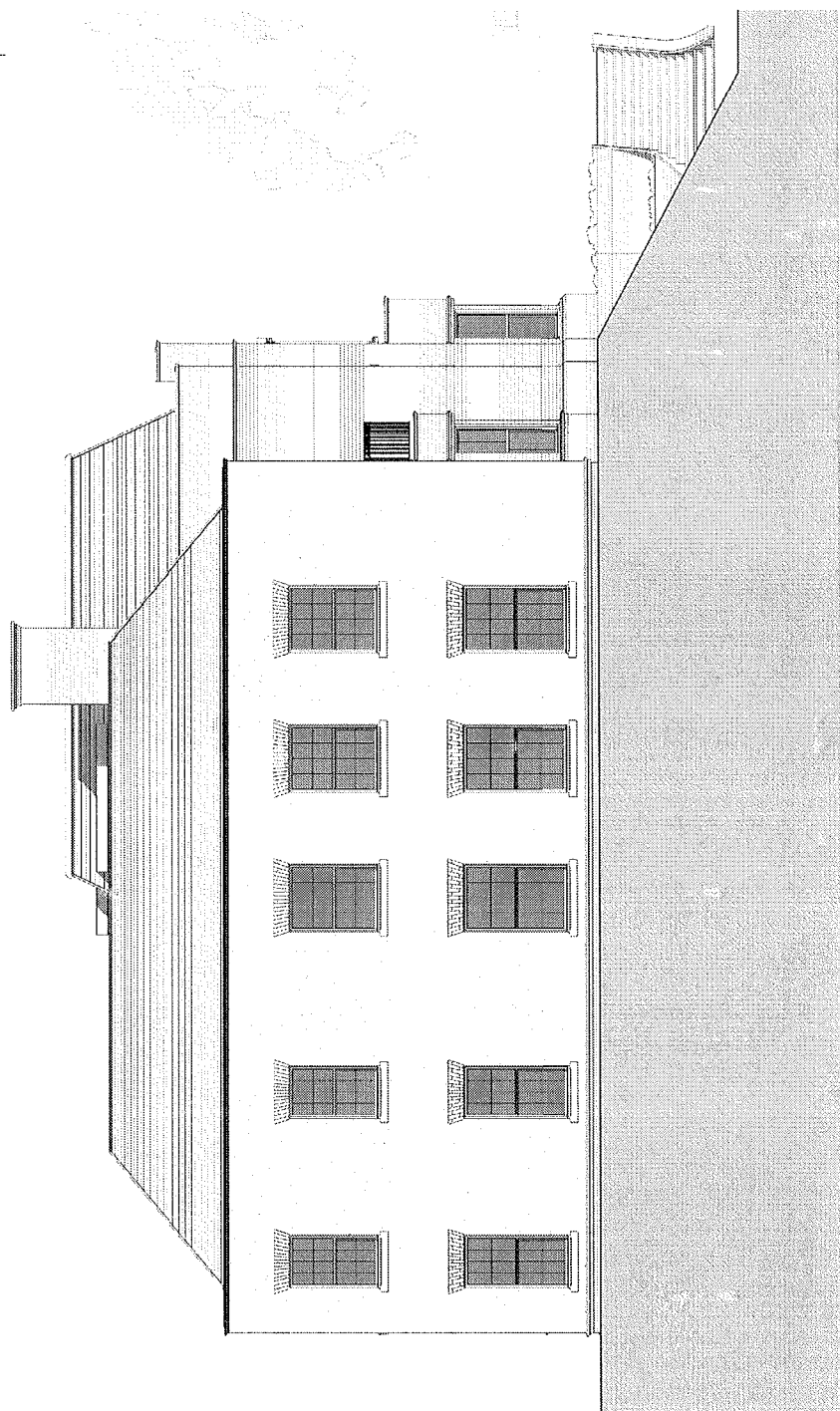


Status: PLANNING
Scale: 1 : 100
Date: April 2016
Revision: Sheet: A3
Drng No: FD 14 - 1220 P3 - 110
Not to be used for PLANNING PURPOSE ONLY and not to be used for CONSTRUCTION.

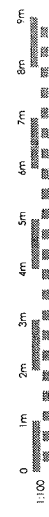
Application 2: 16/01179/FULL

Client: KEBBELL DEVELOPMENT LTD.
Project: Land at Hill House, Cross Road, Summingdale
Title: Front Elevation

Rev	Description	Date



LEFT FLANK ELEVATION

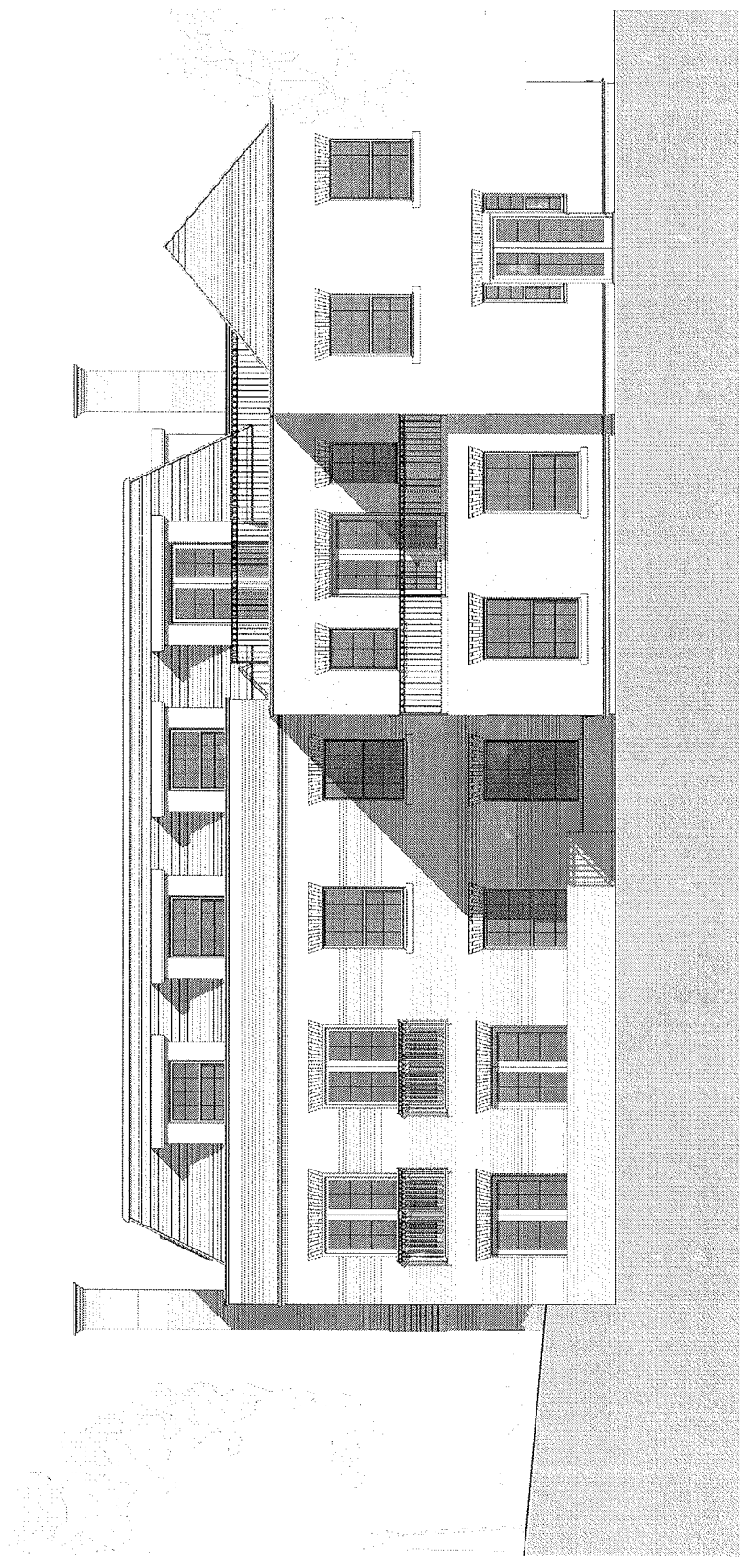


Status: PLANNING
Scale: 1 : 100
Date: April 2016
Revision: Sheet: A3
Dwg No: FD 14 - 1220 P3 - 111
Warning: This drawing is produced for PLANNING PURPOSE ONLY and is not to be used for construction.

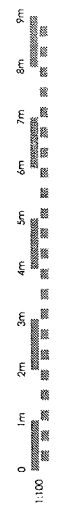
Application 2: 16/01179/FULL

Client: KEBBELL DEVELOPMENT LTD.
Project: Land at Hill House, Cross Road, Sunningdale
Title: Left Flank Elevation

Rev	Description	Date



REAR ELEVATION



Status: PLANNING

Scale: 1 : 100

Date: April 2016

Revision: A3

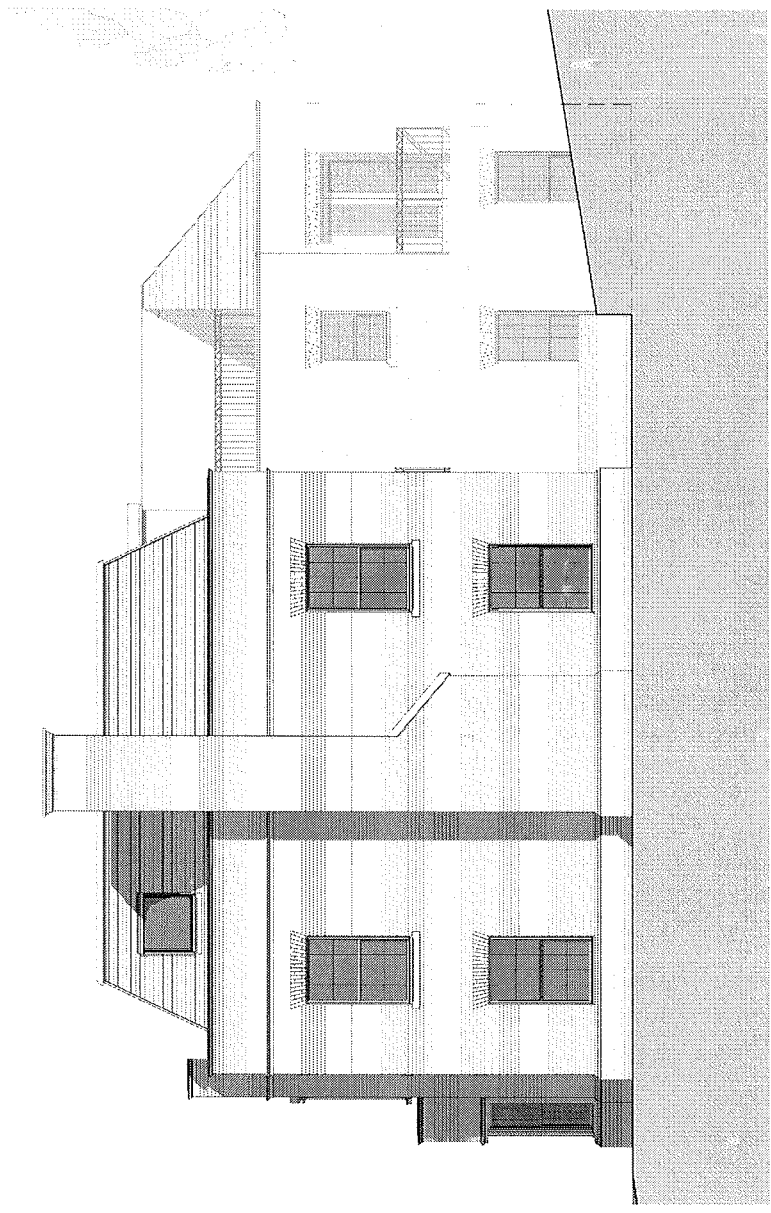
Drng No: FD 14 - 1220 P3 - 112

Planning
Drawing
Produced for PLANNING PURPOSE ONLY and is not to be used for construction.

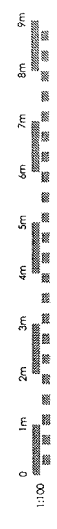
Application 2: 16/01179/FULL

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Rear Elevation

Rev	Description	Date



RIGHT FLANK ELEVATION



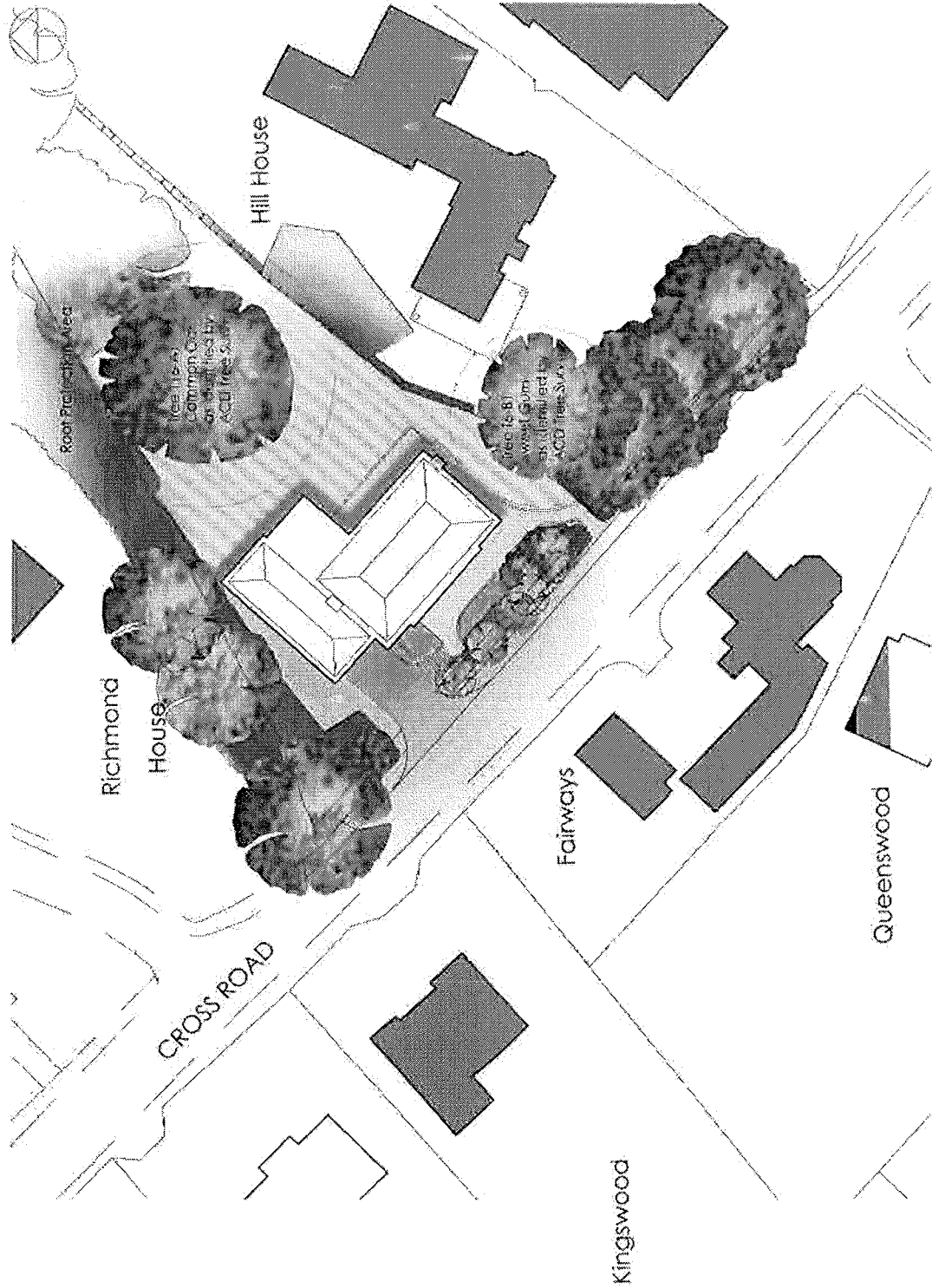
Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Right Flank Elevation

Application 2: 16/01179/FULL

Status: PLANNING
 Scale: 1 : 100
 Date: April 2016
 Revision: A3
 Sheet: A3
 Drg No: FD 14-1220 P3 - 113
 Planning
 This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

APPENDIX D.

No.	Description	Date
1	Approved by Planning Commission	10/04/13
2	Approved by Planning Commission	10/04/13
3	Approved by Planning Commission	10/04/13
4	Approved by Planning Commission	10/04/13
5	Approved by Planning Commission	10/04/13
6	Approved by Planning Commission	10/04/13
7	Approved by Planning Commission	10/04/13
8	Approved by Planning Commission	10/04/13
9	Approved by Planning Commission	10/04/13
10	Approved by Planning Commission	10/04/13
11	Approved by Planning Commission	10/04/13
12	Approved by Planning Commission	10/04/13
13	Approved by Planning Commission	10/04/13
14	Approved by Planning Commission	10/04/13
15	Approved by Planning Commission	10/04/13
16	Approved by Planning Commission	10/04/13
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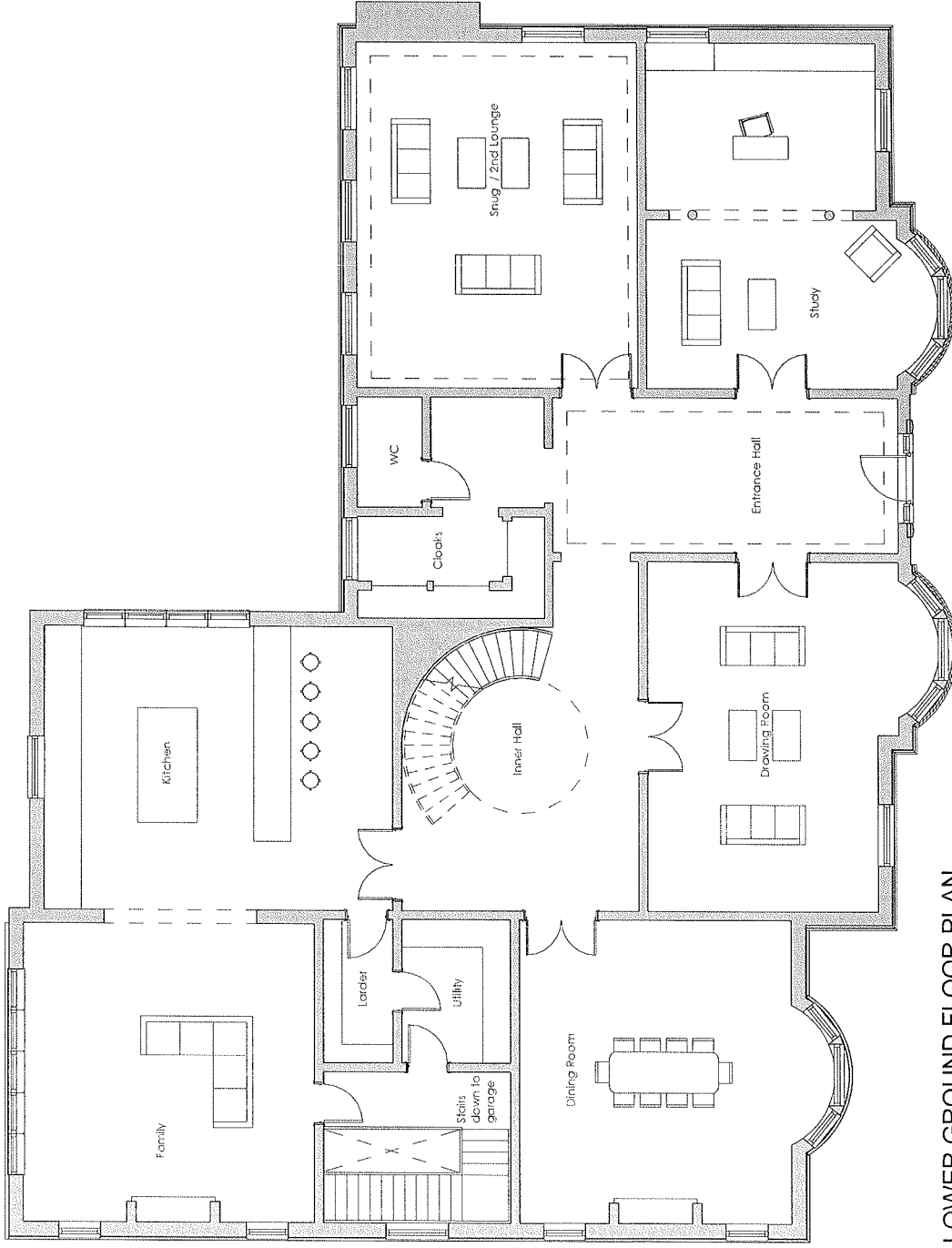


Status: PLANNING
Scale: 1:500
Revision: C
Date: 02/08/13
Sheet: A3
Drwg No: FSW 13-1031-55

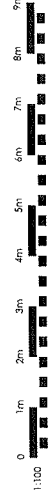
Extant permission: 13/01206/FULL

Client: MR AND MRS WHEATON
Project: Land at Hill House, Cross Road, Surringdale
Title: Proposed Site Layout

Rev	Description	Date
A	Amended further to client comments and scheme progression	16.04.13
B	Amended further to client comments	17.04.13
C	Amended further to client comments	29.04.13
D	Addition of kitchen window	29.04.13
E	Change of window	30.04.13
F	Change of window	24.07.13
G	Change of window Closets to be placed as requested by client Kitchen amended, balcony removed, chimney moved	24.07.13



LOWER GROUND FLOOR PLAN



Status: PLANNING

Scale: 1 : 100

Date: 02/28/13

Revision: G

Sheet: A3

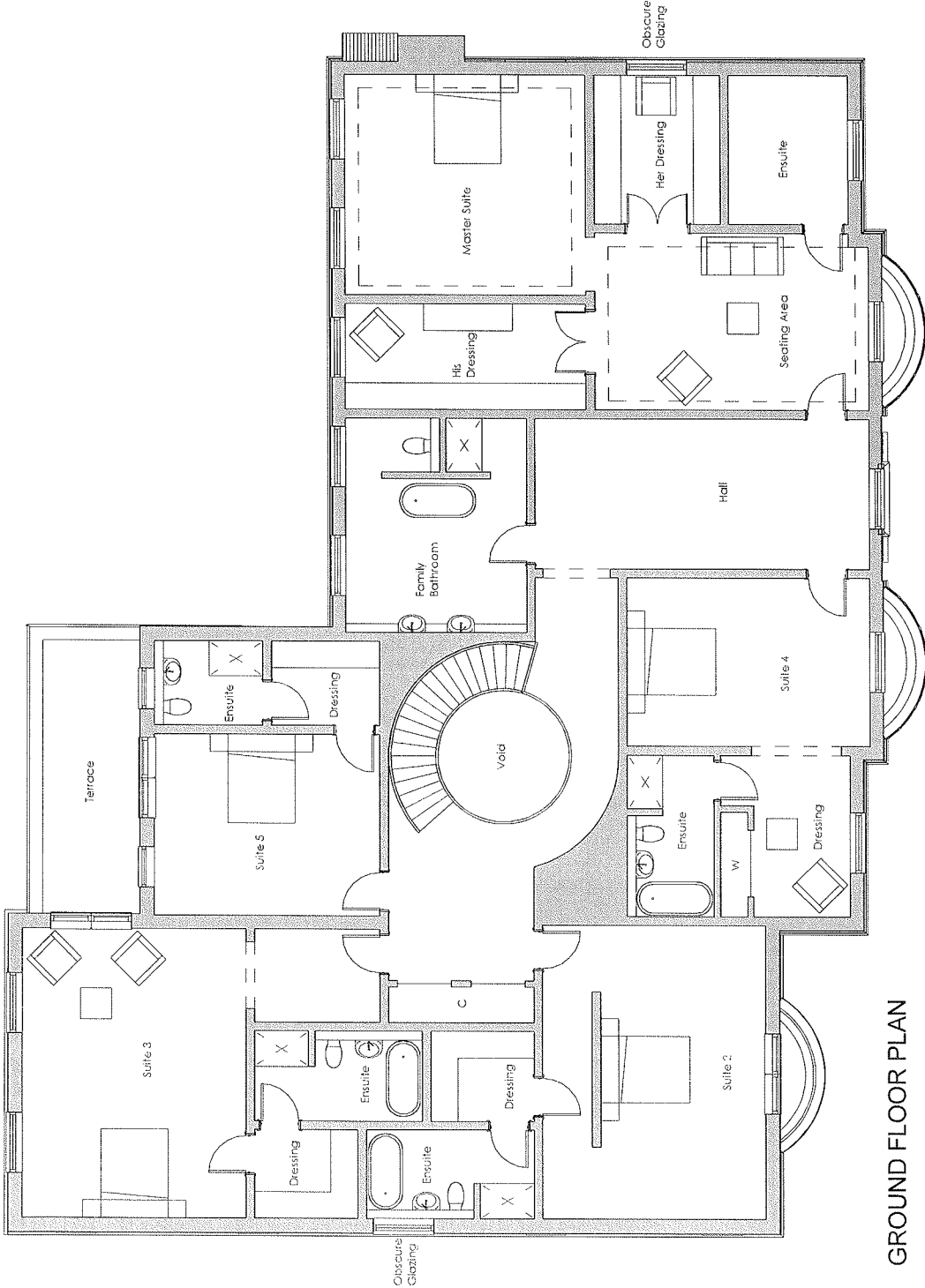
Drng No: FSW 13 - 1031- 101

Planning
This drawing is prepared for PLANNING PURPOSES ONLY and is not to be used for construction.

Exant permission: 13/01206/FULL

Client: MR AND MRS WHEATON
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Lower Ground Floor Plan

Rev	Description	Date
A	Amended further to client comments and scheme progression	16.04.13
B	Amended further to client comments	17.04.13
C	Amended further to client comments	29.04.13
D	Kitchen amended, balcony removed, chimney moved	24.07.13



GROUND FLOOR PLAN

Status: PLANNING

Scale: 1 : 100

Date: 02/28/13

Revision: D

Sheet: A3

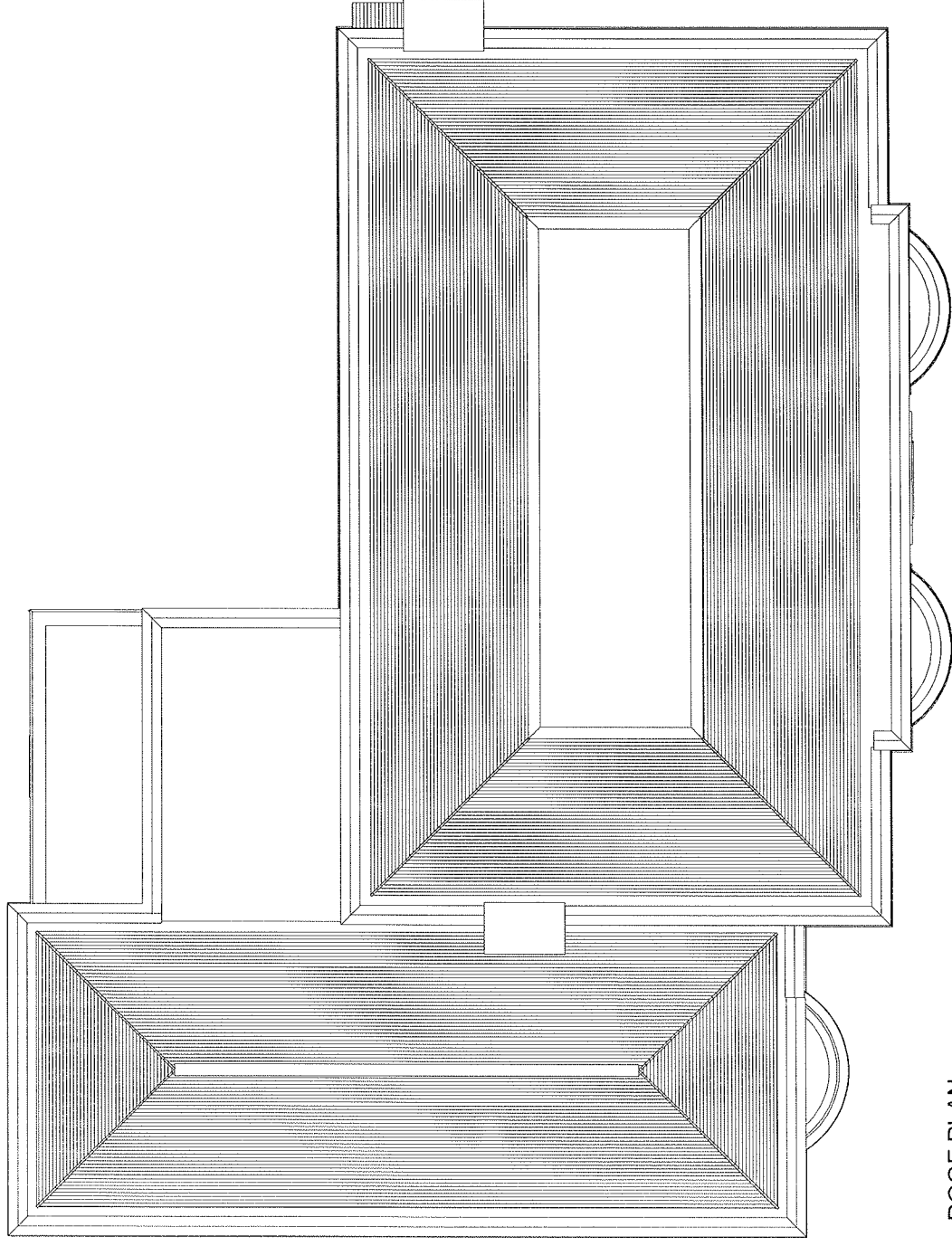
Drng No: FSW 13 - 1031- 102

Planning
This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

Exant permission: 13/01206/FULL

Client: MR AND MRS WHEATON
Project: Land at Hill House, Cross Road, Sunningdale
Title: Ground Floor Plan

Rev / Description / Date
 A / Kitchen amended, balcony removed, chimney moved / 24.07.13



ROOF PLAN



Status: PLANNING

Scale: 1 : 100

Date: 02/28/13

Revision: A

Sheet: A3

Drwg No: FSW 13 - 1031- 104

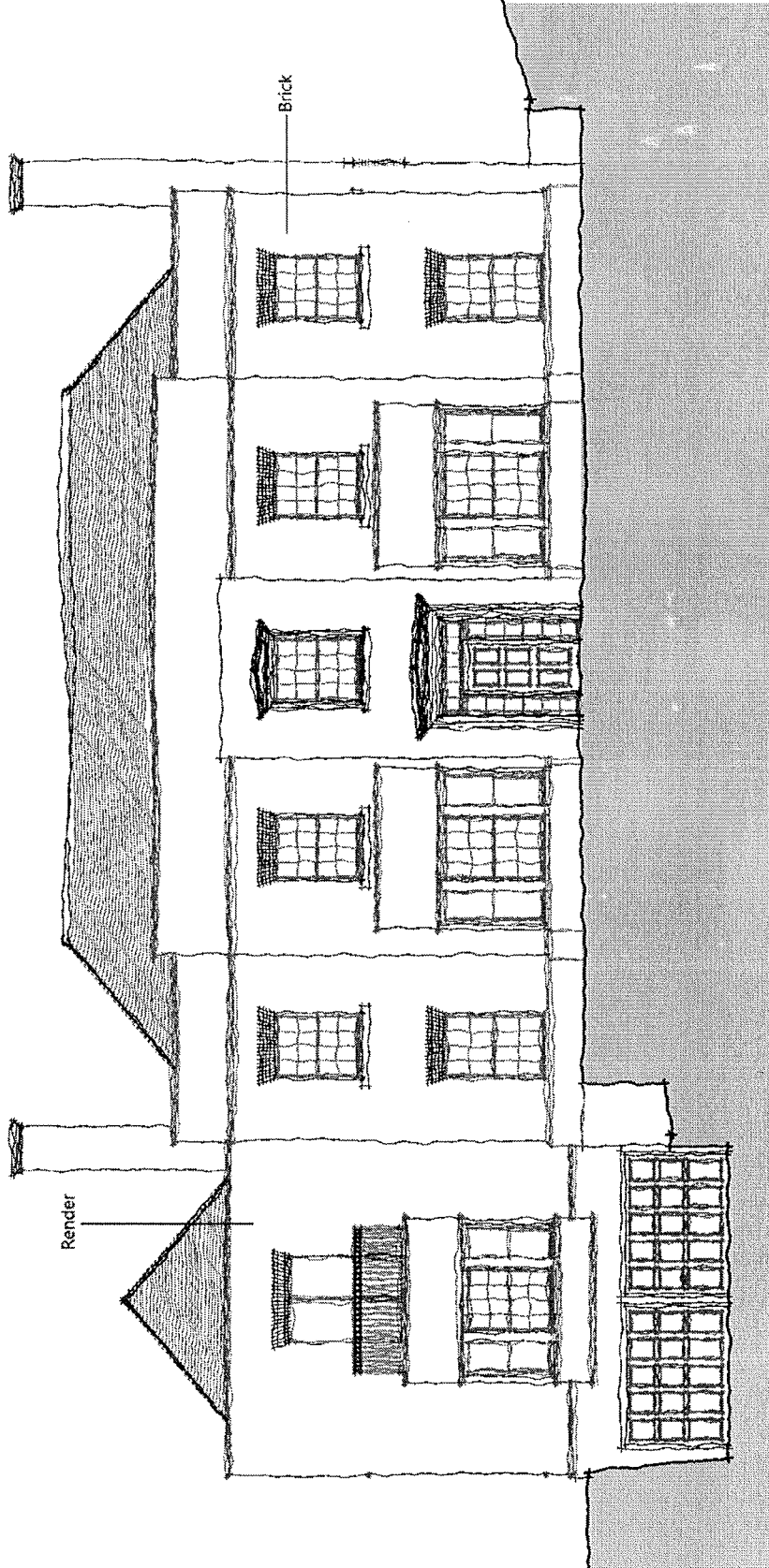
This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

13/01206/FULL

Extant permission:

Client: MR AND MRS WHEATON
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Roof Plan

Rev.	Description	Date
A	Amended further to client comments and scheme progression	16.04.13
B	Amended further to client comments	17.04.13
C	Amended further to client comments	17.04.13



FRONT ELEVATION

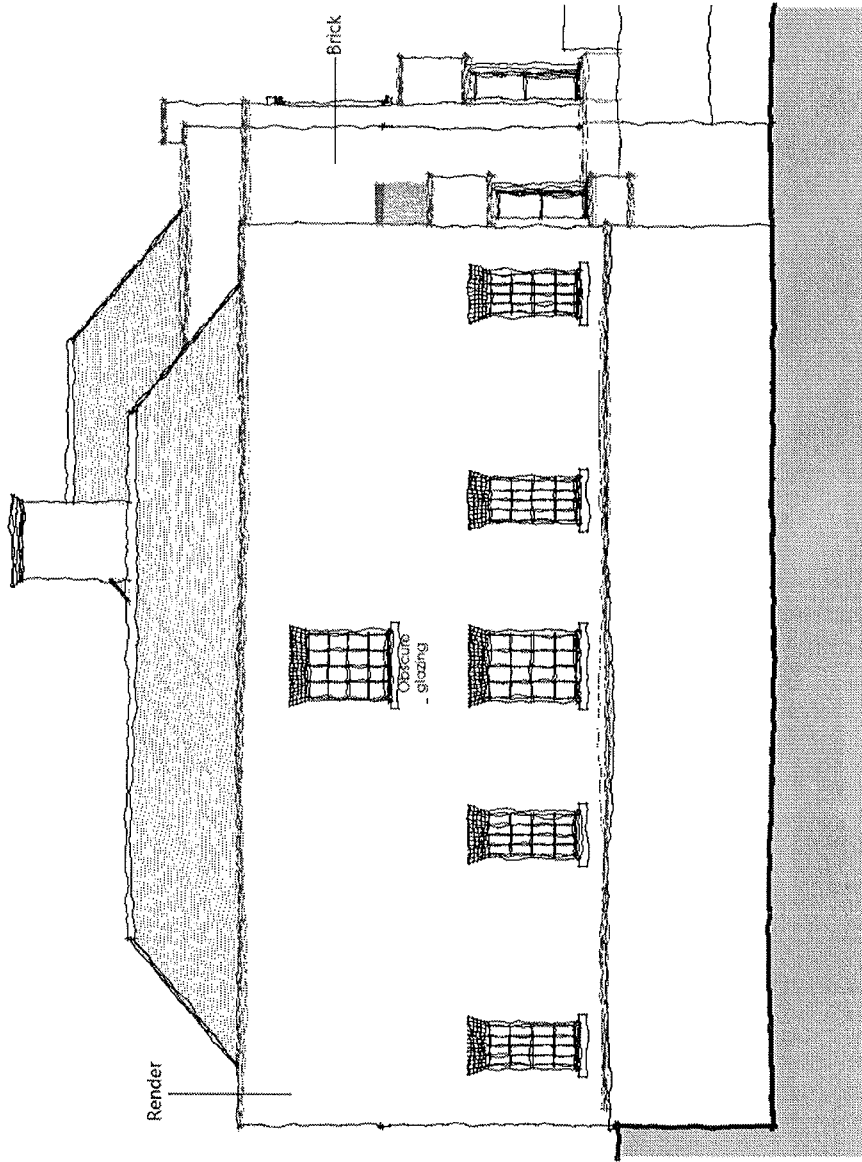
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 Revision: C
 Date: 02/28/13
 Sheet: A3
 Dwg No: FSW 13 - 103F 110

Extant permission: 13/01206/FULL

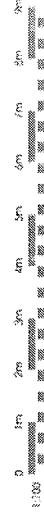
Client: MR AND MRS WHEATON
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Front Elevation

This drawing is intended for PLANNING PURPOSES ONLY and is not to be used for construction.

Rev	Description	Date
A	Approved further to client comments and scheme	16.04.13
B	Approved further to client comments	17.04.13
C	Approved further to client comments	29.04.13



LEFT FLANK ELEVATION



Status: PLANNING

Scale: 1:100

Date: 02/28/13

Revision: C

Sheet: A3

Dwg No: FSW 13 - 103F-111

Planning

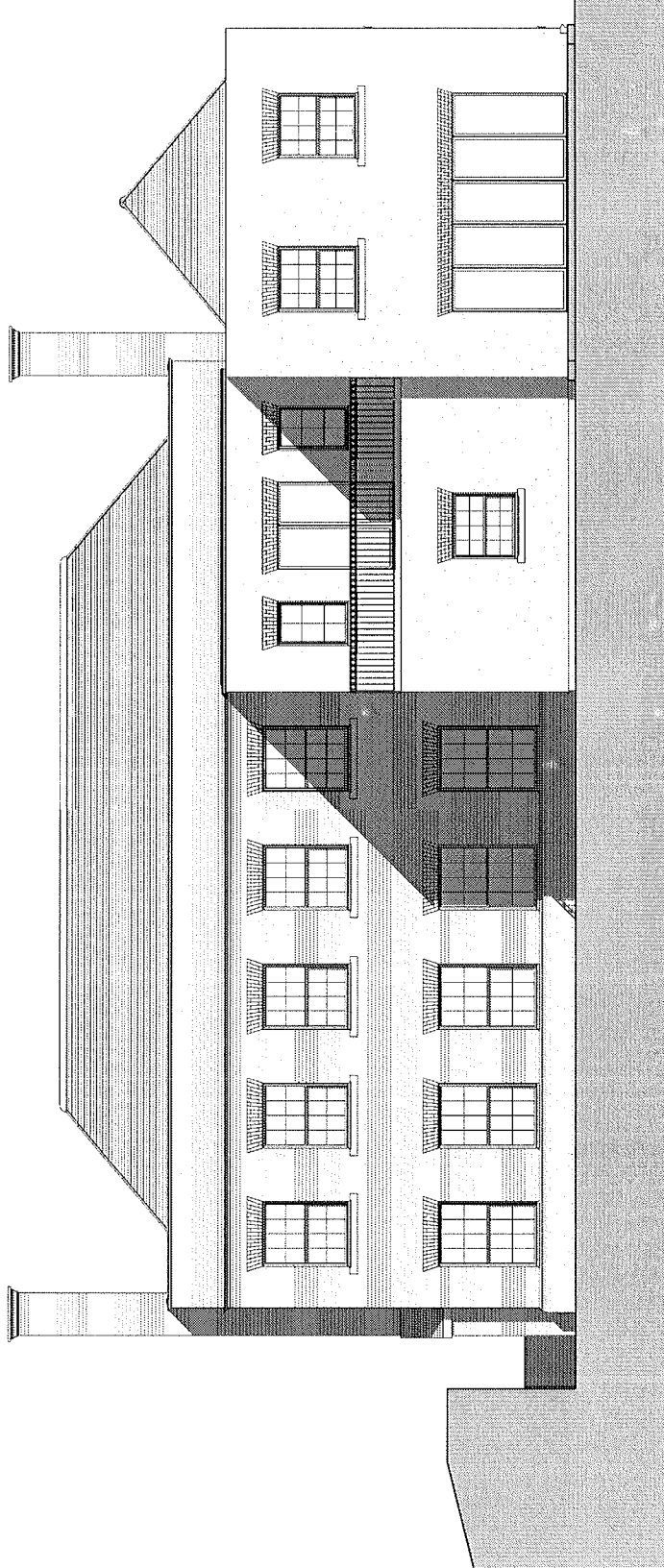
This drawing is prepared for PLANNING PURPOSES ONLY and is not to be used for construction.

13/01206/FULL

Exant perm: Non:

Client: MR AND MRS WHEATON
 Project: Land at Hill House, Cross Road, Summingdale
 Title: Left Flank Elevation

Rev.	Description	Date
A	Amended further to client comments and scheme progression	16.04.13
B	Amended further to client comments	17.04.13
C	Doors added to kitchen as requested by council	24.07.13
D	Kitchen amended, balcony removed, chimney moved	24.07.13



REAR ELEVATION

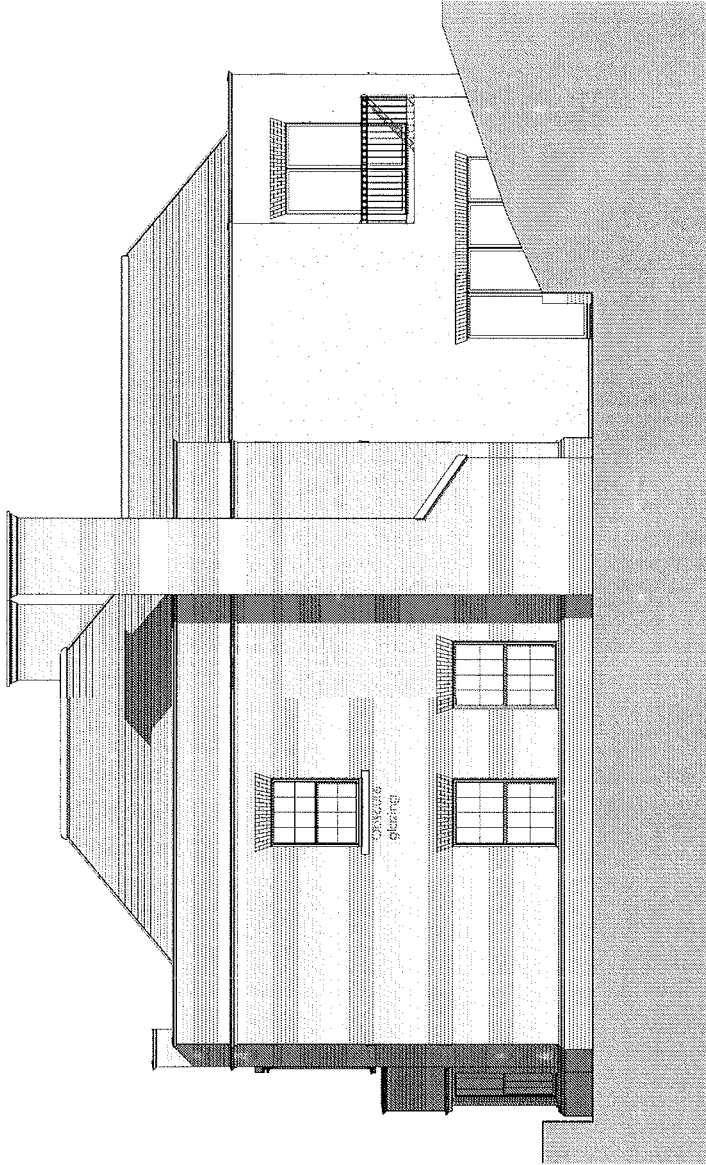


Status: PLANNING
Scale: 1 : 100
Date: 02/28/13
Revision: D
Sheet: A3
Dwg No: FSW 13 - 1031-112
Planning permission is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

Client: MR AND MRS WHEATON
Project: Land at Hill House, Cross Road, Sunningdale
Title: Rear Elevation

Extant permission: 13/01206/FULL

Rev.	Description	Date
A	Amended further to client comments and scheme progression	16.04.13
B	Amended further to client comments	17.04.13
C	Amended further to client comments	28.04.13
D	Addition of lounge / lounge windows	30.04.13
E	Kitchen amended, balcony removed, chimney moved	24.07.13



RIGHT FLANK ELEVATION



Client: MR AND MRS WHEATON
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Right Flank Elevation

Exeat permission: 13/01206/FULL

Status: PLANNING

Scale: 1 : 100

Date: 02/28/13

Revision: E

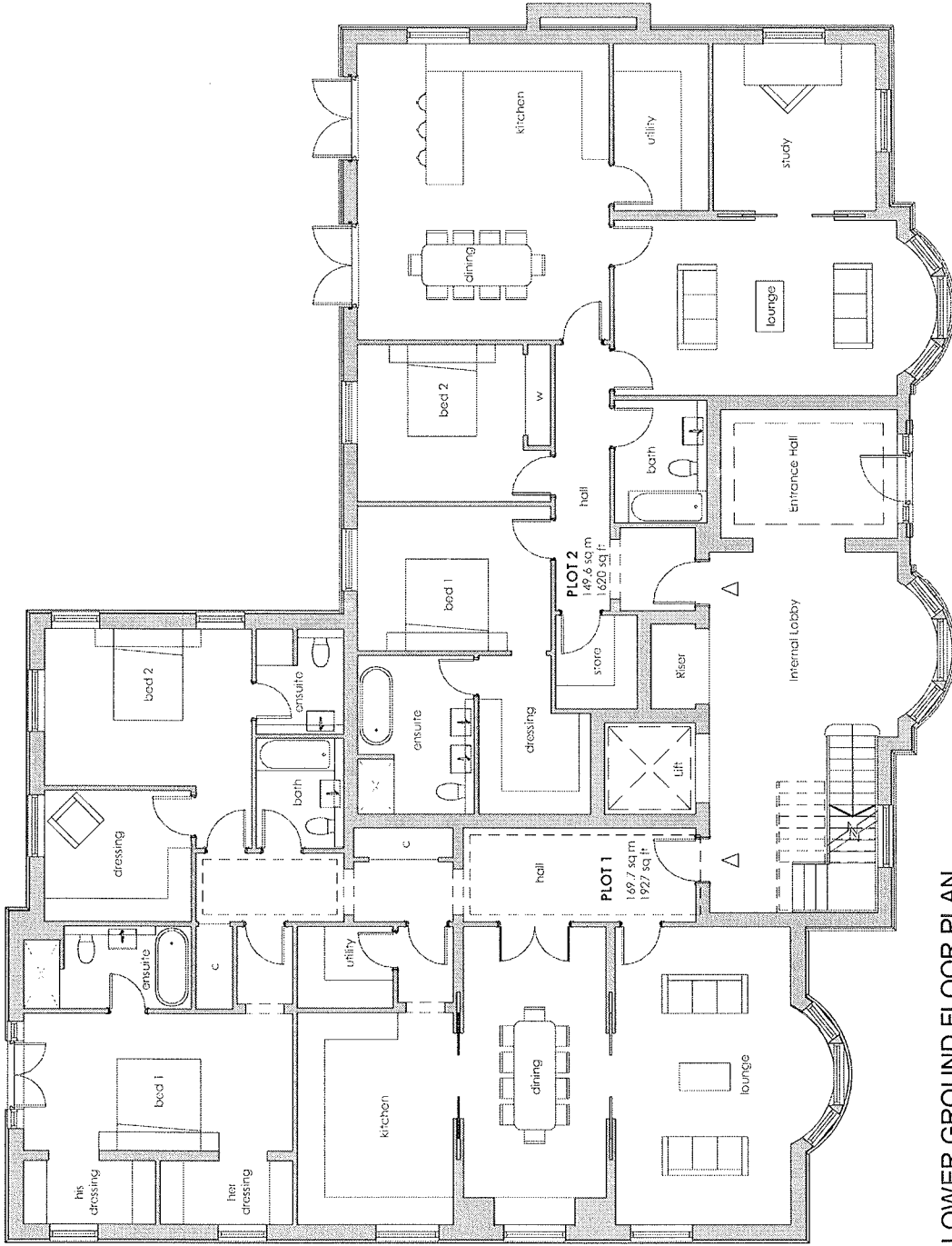
Sheet: A3

Drwg No: FSW 13 - 1031- 113

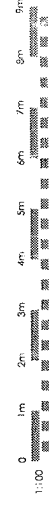
Planning
 Permission is processed for PLANNING PURPOSES ONLY and is not to be used for
 construction.

APPENDIX E.

Rev.	Description	Date
A	Amendments made in line with clients comments received 16.01.15	18.01.15



LOWER GROUND FLOOR PLAN



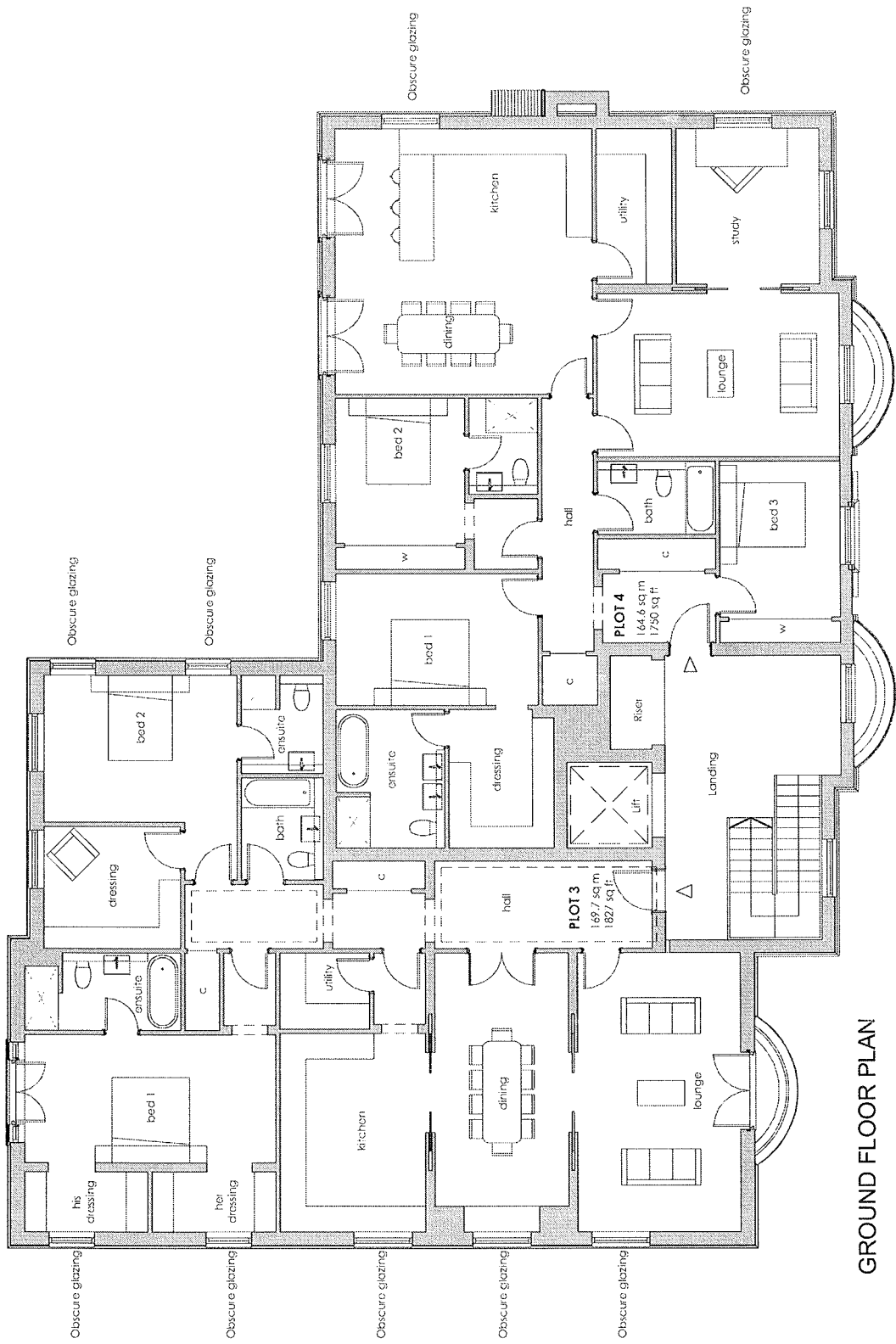
14/03591/FULL

Status: PLANNING
Scale: 1 : 100
Revision: A
Drwg No: FSW 14 - 1220-101
Date: Oct 2014
Sheet: A3

Client: MR AND MRS WHEATON
Project: Land at Hill House, Cross Road, Sunningdale
Title: Lower Ground Floor Plan

This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

Rev	Description	Date
A	Amendments further to clients comments	16.10.14
B	Amendments made further clients comments received	20.10.14
C	Amendments made in line with clients comments received	19.01.15



GROUND FLOOR PLAN

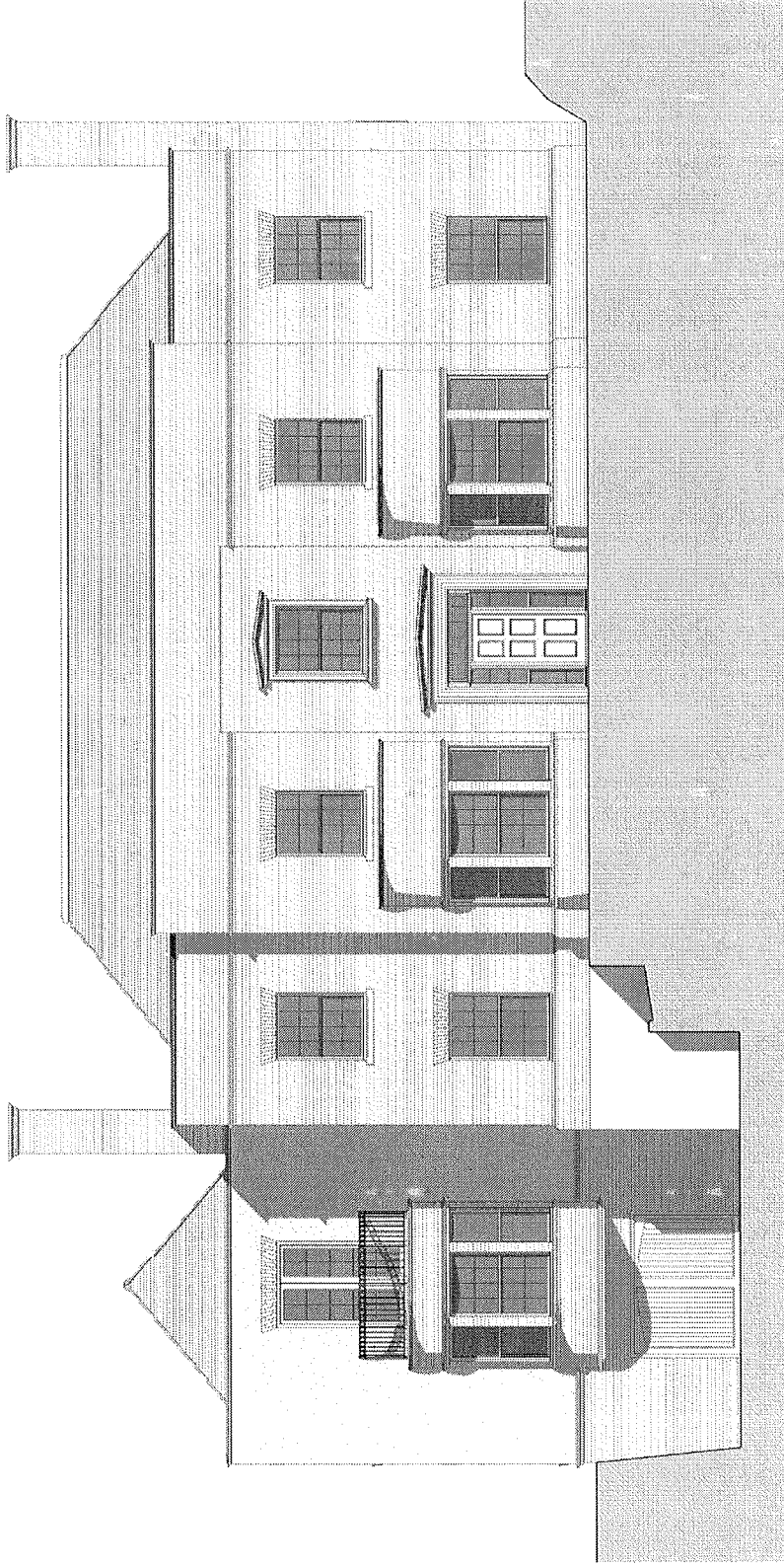
14/03591/FULL

Status: PLANNING
 Scale: 1 : 100
 Revision: C
 Date: Oct 2014
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 Dwg No: FSW 14 - 1220-102

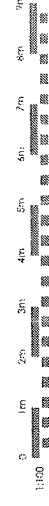
Client: MR AND MRS WHEATON
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Ground Floor Plan

This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

Rev.	Description	Date
A	Amendments made further client comments received	28.10.14
B	Final approved	10.11.14



FRONT ELEVATION

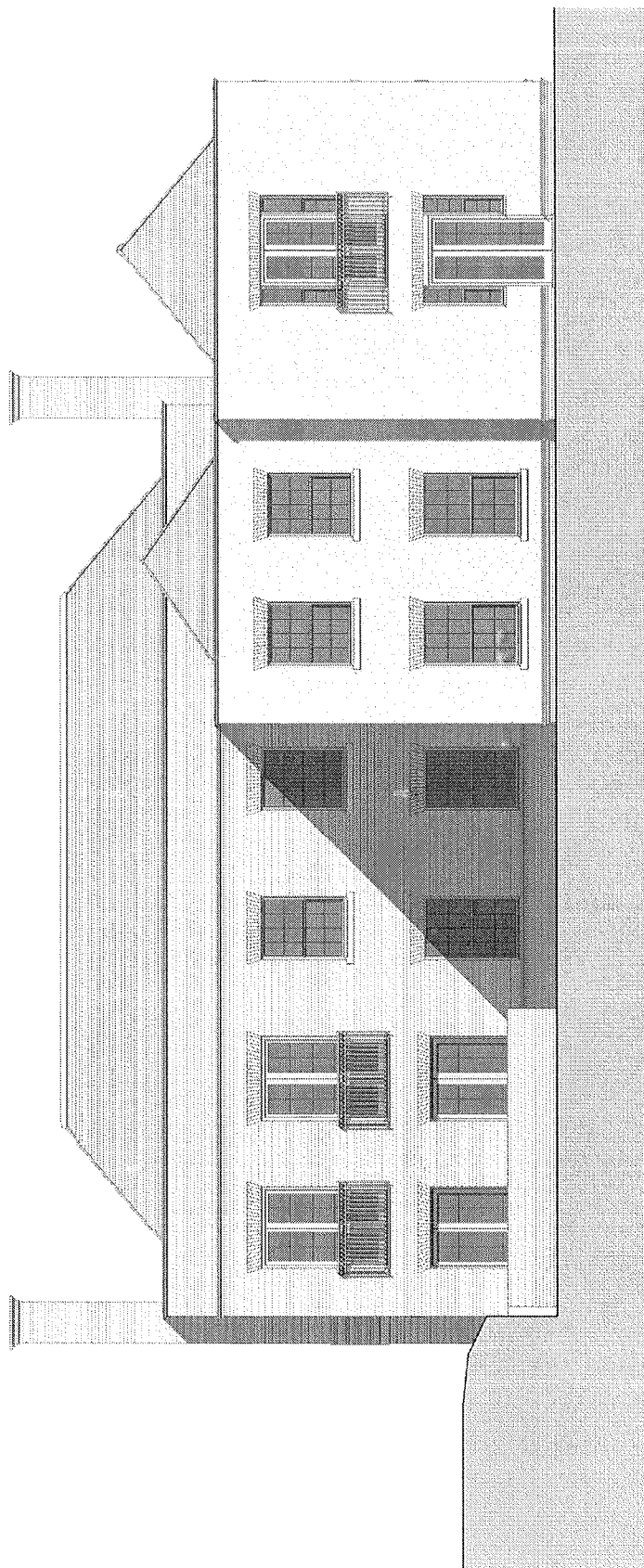


14/03591/FULL

Client: MR AND MRS WHEATON
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Front Elevation

Status: PLANNING
 Scale: 1 : 100
 Revision: B
 Date: Oct 2014
 Sheet: A3
 Dwg No: FSW 14 - 1220-110
 Planning
 This sheet is prepared to illustrate the proposed development.

Rev	Description	Date
A	Amendments further to clients comments.	16.10.14
B	Plan removed	10.11.14



REAR ELEVATION

14/03591/FULL

Client: MR AND MRS WHEATON
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Rear Elevation

Status: PLANNING

Scale: 1:100

Date: Oct 2014

Revision: B

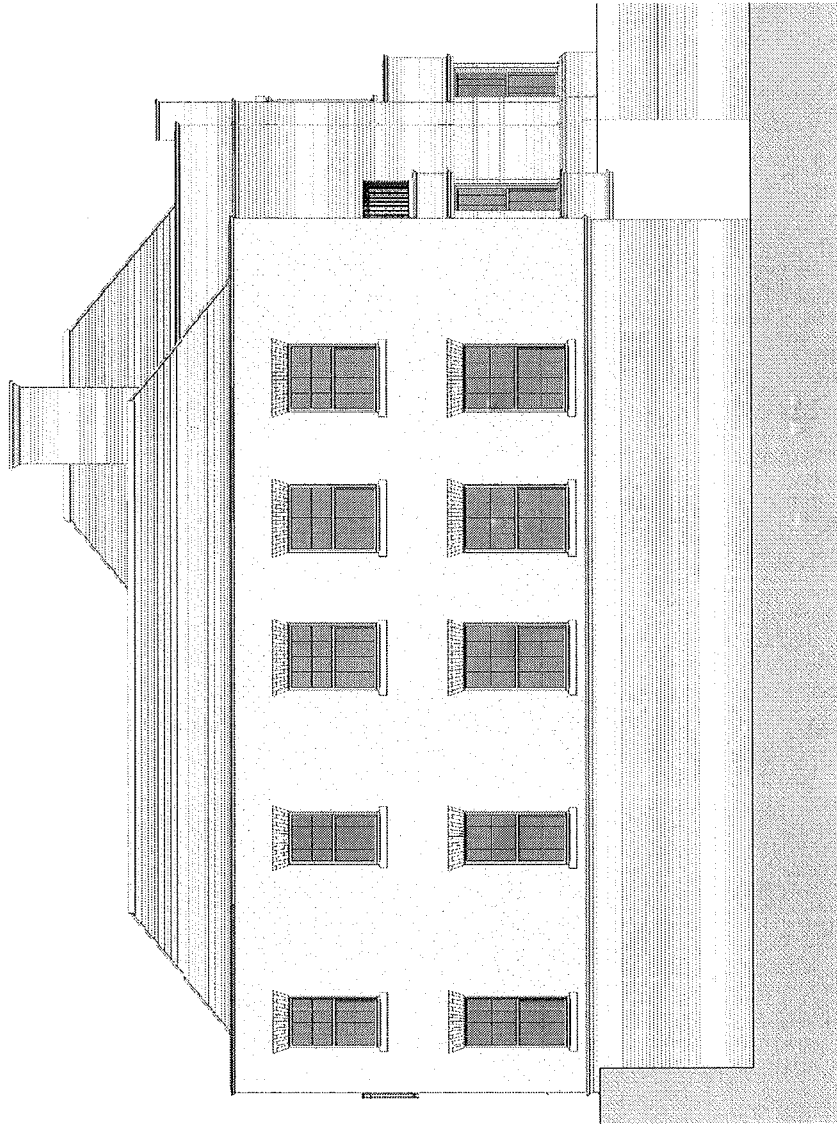
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Drwg No: FSW 14 - 1220-112

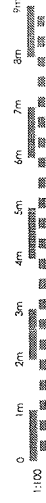
Planning
 14/03591/FULL

1:100
 0 1m 2m 3m 4m 5m 6m 7m 8m 9m

Rev	Description	Date
A	Amendments made further clients comments received	20.10.14
B	Amendments made in line with clients comments received	19.01.15



LEFT FLANK ELEVATION



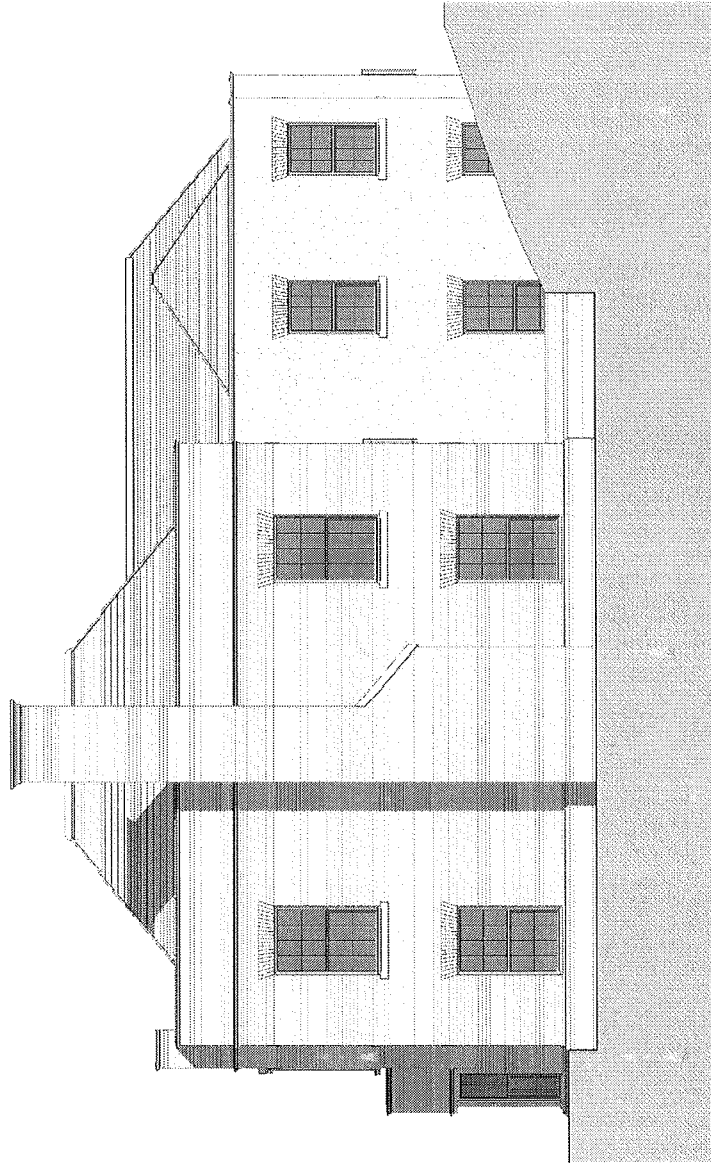
14/03591/FULL

Status: PLANNING
Scale: 1 : 100
Revision: B
Revision: A3
Date: Oct 2014
Sheet: A3
Drwg No: FSW 14 - 1220-111

Client: MR AND MRS WHEATON
Project: Land at Hill House, Cross Road, Sunningdale
Title: Left Flank Elevation

Notes:

Rev.	Description	Date
A	Amendments further to clients comments	16.10.14
B	Plan removed	10.11.14
C	Amendments made in line with clients comments received	19.01.15
		16.01.15



RIGHT FLANK ELEVATION



14/03591/FULL

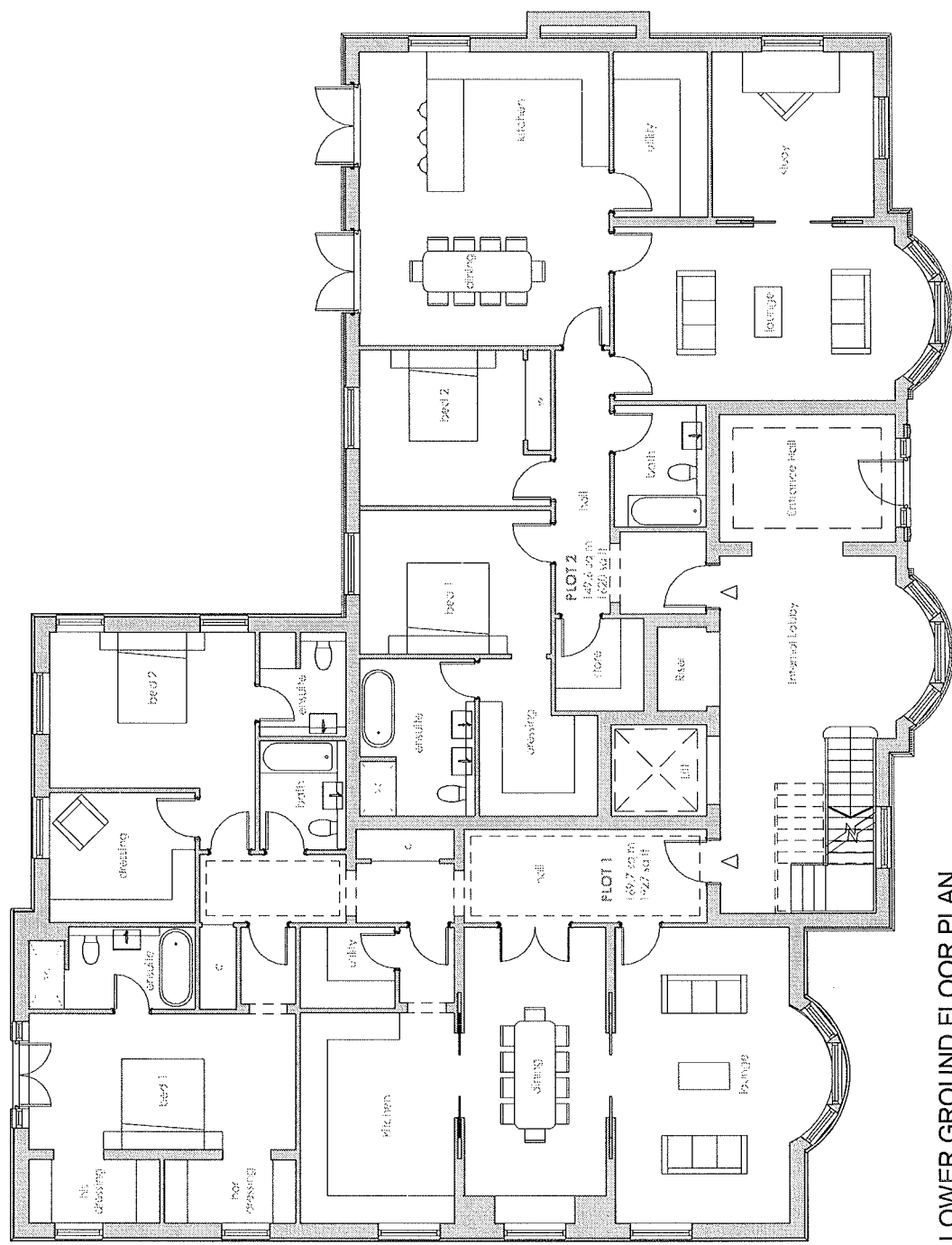
Status: PLANNING
 Scale: 1 : 100
 Revision: C
 Drg No: FSW 14 - 1220-113

Client: MR AND MRS WHEATON
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Right Flank Elevation

Date: Oct 2014
 Sheet: A3
 Planning
 The above is prepared for PLANNING PURPOSES ONLY and is not to be used for construction.

APPENDIX F.

Rev	Description	Date
A	Amendments made in line with clients comments received 16.01.15	19.01.15
B	Amendment in roof added	02.06.15



LOWER GROUND FLOOR PLAN

15/01199/FULL

Status: PLANNING
 Scale: 1:100
 Revision: B
 Dwg No: FSW 14-1220-101

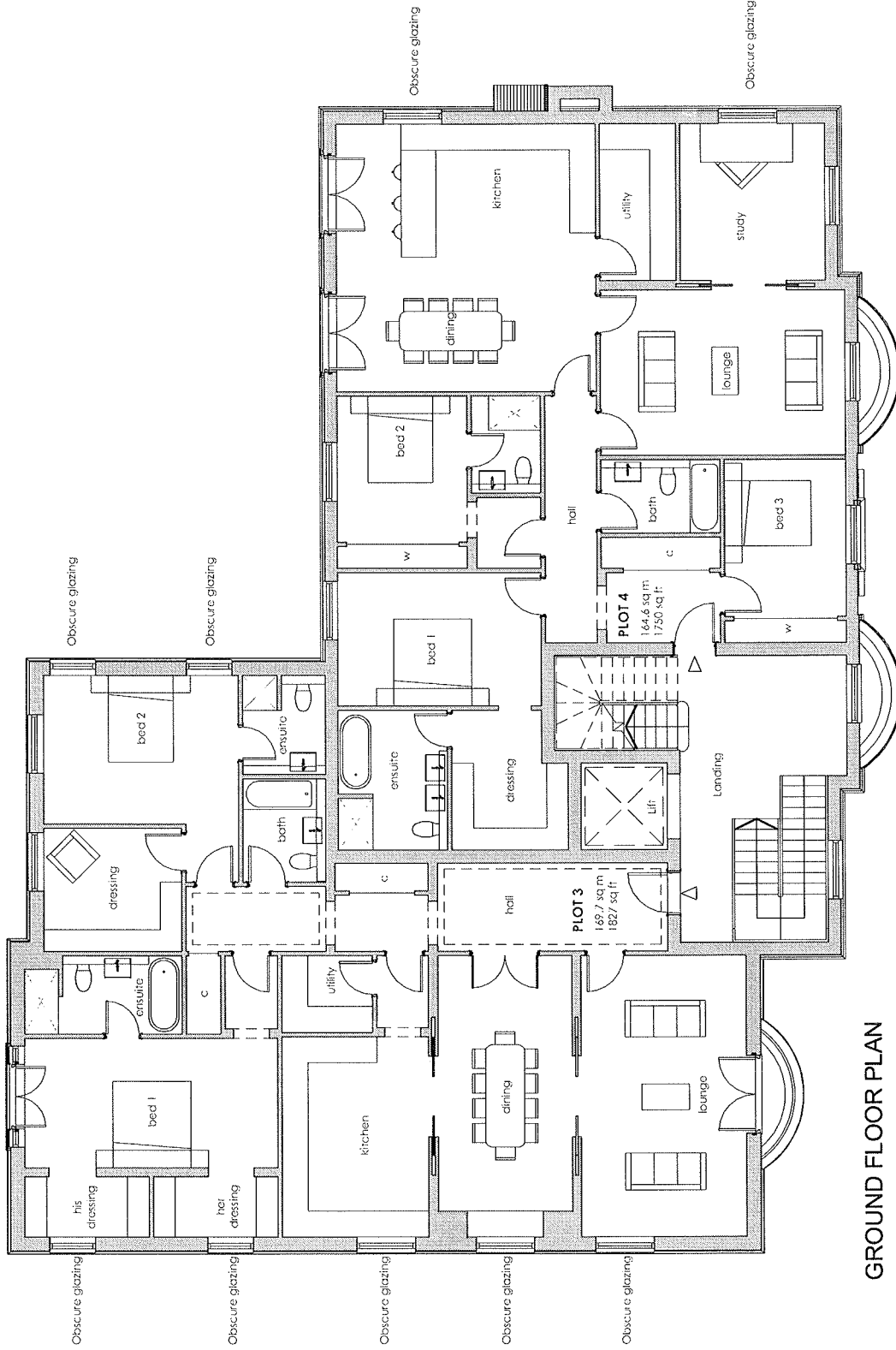
Date: Oct 2014
 Sheet: A3

This amended amendment for PLANNING PURPOSES ONLY, must not be used for construction.

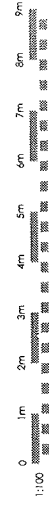
Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Lower Ground Floor Plan

Notes:

Rev.	Description	Date
A	Amendments turner to clients comments	16.10.14
B	Amendments made further clients comments received	20.10.14
C	Amendments made in line with clients comments received	19.01.15
D	Apartment in roof added	02.03.15



GROUND FLOOR PLAN



Status: PLANNING

Scale: 1 : 100

Date: Oct 2014

Revision: D

Sheet: A3

Drwg No: FSW 14 - 1220-102

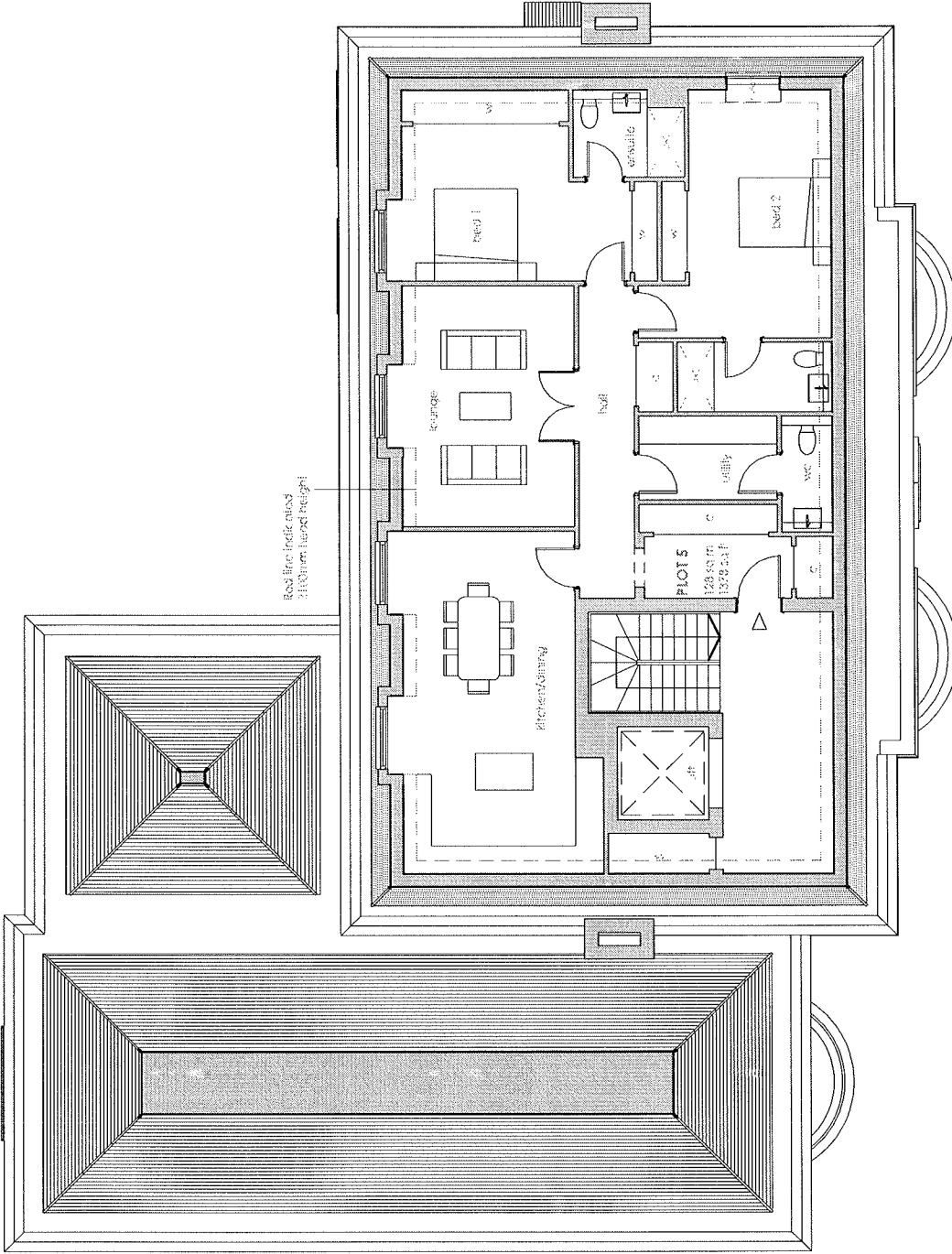
This drawing is produced for PLANNING PURPOSES ONLY and is not to be used for construction.

15/01199/FULL

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Ground Floor Plan

Rev. Description
 A Apartment in roof added

Date:
 02.03.15



FIRST FLOOR PLAN

Status: PLANNING

Scale: 1 : 100

Date: 03/02/15

Revision: A

Sheet: A3

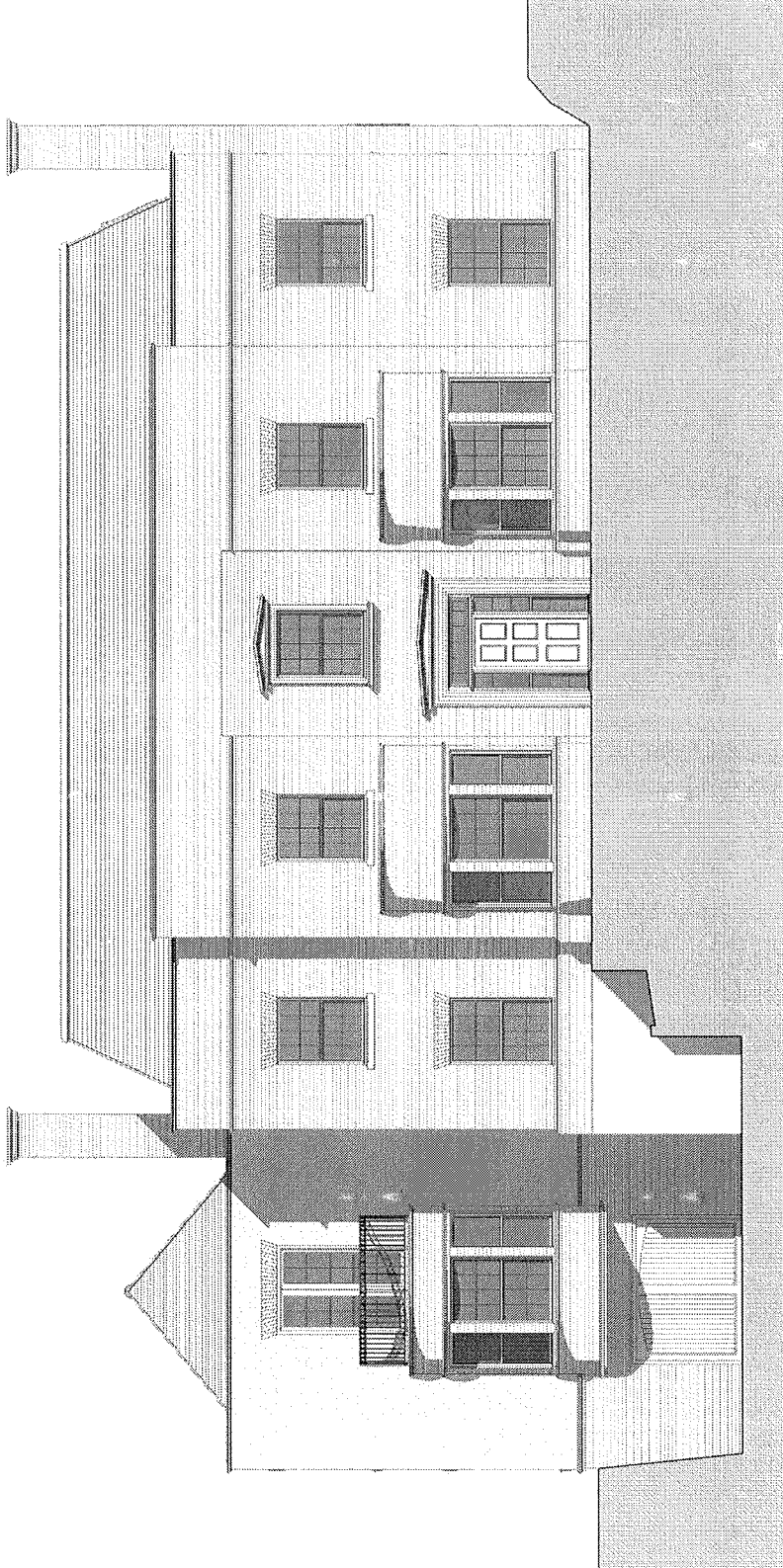
Drwg No: FSW 14 - 1220-103

This drawing is prepared for PLANNING PURPOSES ONLY and is not valid for construction.

15/01199/FULL

Client: KEBBELL DEVELOPMENT LTD.
Project: Land at Hill House, Cross Road, Sunningdale
Title: First Floor Plan

Rev	Description	Date
A	Amendments made further clients comments received	20.10.14
B	Path removed	10.11.14
C	Apartment in roof added	02.03.15



FRONT ELEVATION

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Front Elevation

Status: PLANNING

Scale: 1 : 100

Date: Oct 2014

Revision: C

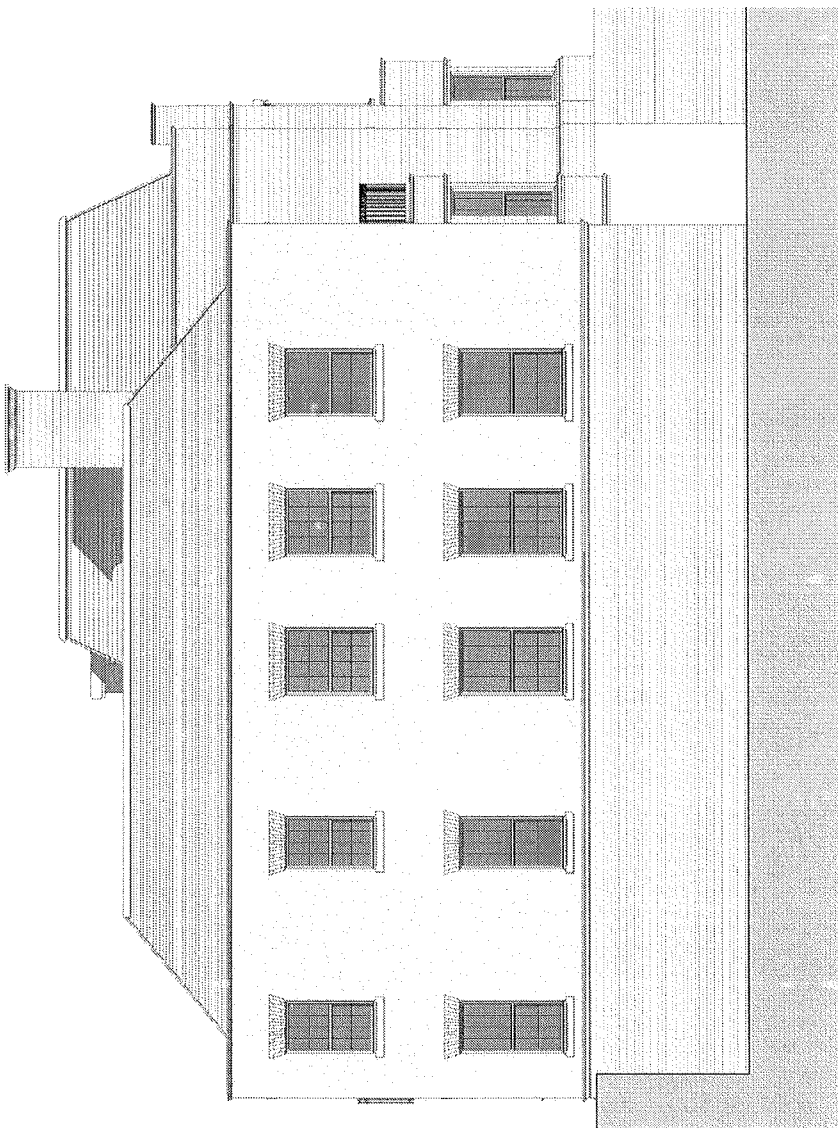
Sheet: A3

Drwg No: FSW 14 - 1220-110

Planning
This drawing is produced for PLANNING PURPOSES ONLY and is not for construction.

15/01199/FULL

Rev.	Description	Date
A	Amendments made further clients comments received	20.10.14
B	Amendments made in line with clients comments received	19.01.15
C	Amendment in roof added	02.03.15



LEFT FLANK ELEVATION

Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Left Flank Elevation

15/01199/FULL

Status: PLANNING

Scale: 1 : 100 Date: Oct 2014

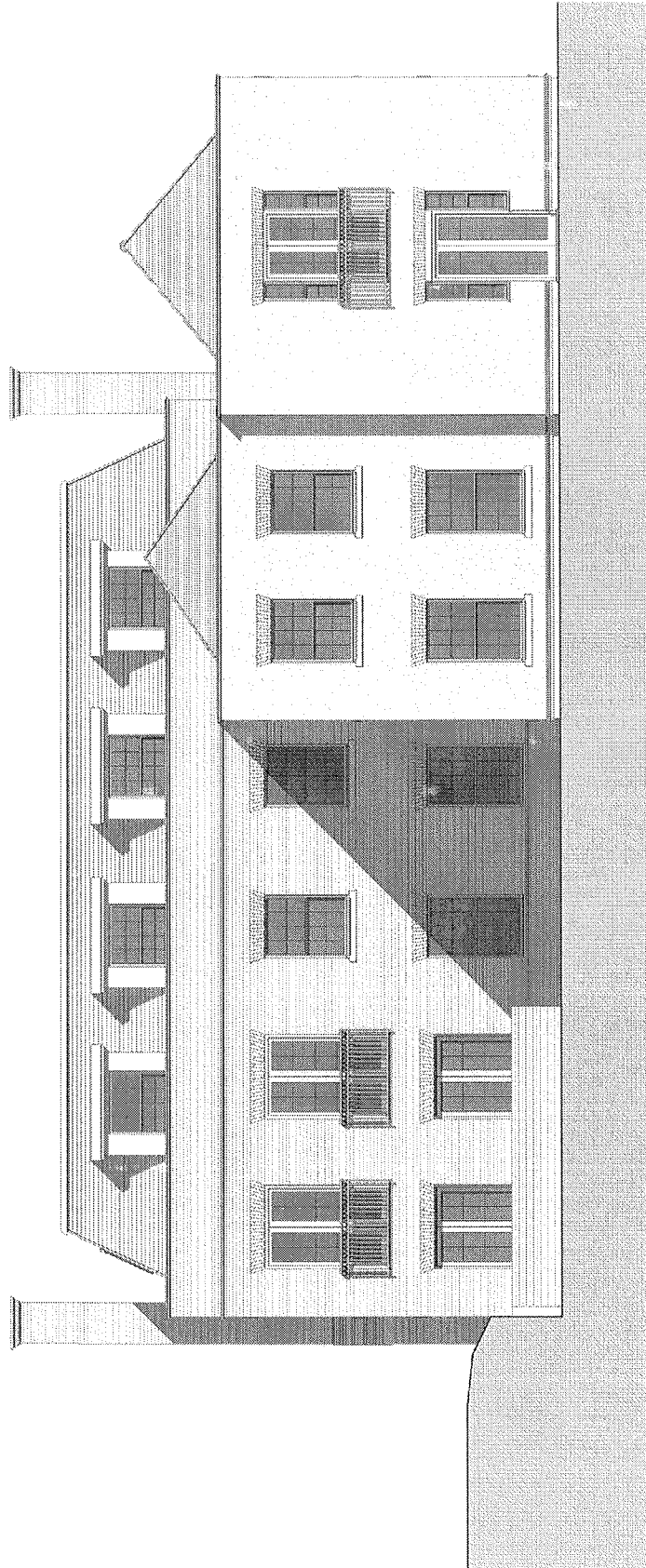
Revision: C Sheet: A3

Drwg No: FSW 14 - 1220-111

Planning
 The drawings are provided for PLANNING PURPOSES ONLY and are not to be used for CONSTRUCTION

Notes:

Rev.	Description	Date
A	Amendments further to clients comments	16.10.14
B	Path removed	10.11.14
C	Apartment in roof added	02.03.15



REAR ELEVATION



Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Rear Elevation

Status: PLANNING

Scale: 1 : 100

Date: Oct 2014

Revision: C

Sheet: A3

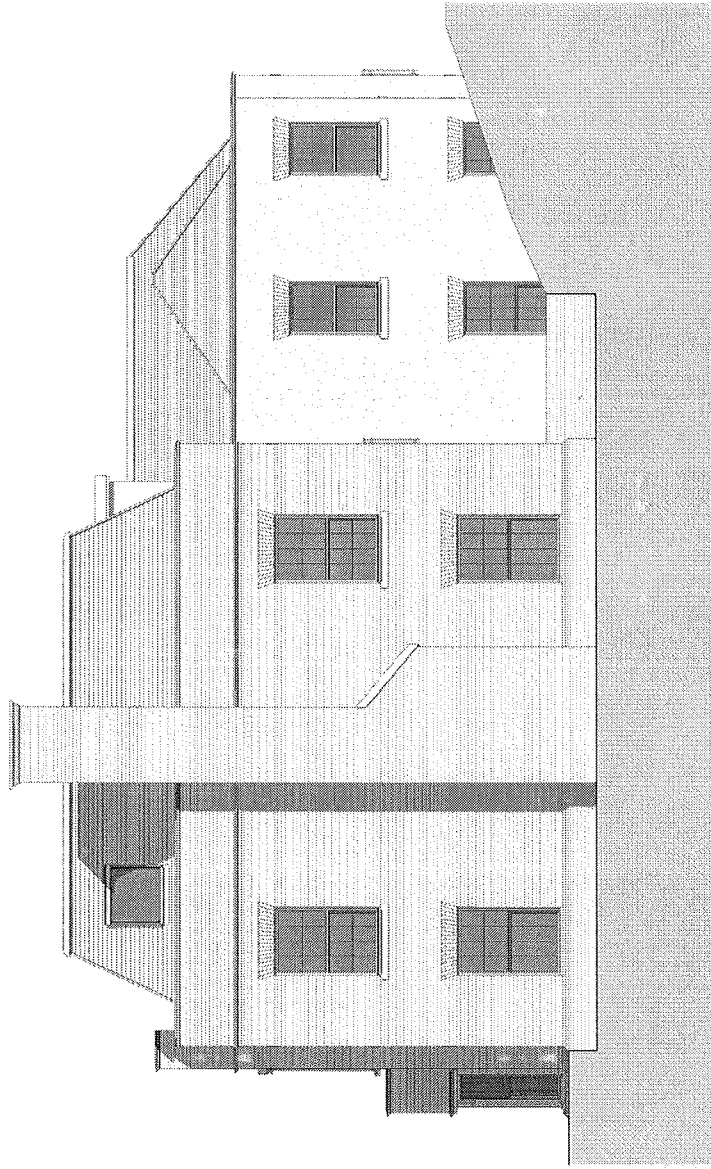
Drwg No: FSW 14 - 1220- 112

Heading

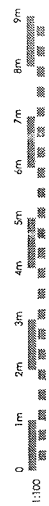
This document is prepared for planning purposes only and is not to be used for construction.

15/01199/FULL

Rev	Description	Date
A	Amendments further to clients comments	16.10.14
B	Path removed	10.11.14
C	Amendments made in line with clients comments received 16.01.15	19.01.15
D	Apartment in roof added	02.03.15



RIGHT FLANK ELEVATION



Client: KEBBELL DEVELOPMENT LTD.
 Project: Land at Hill House, Cross Road, Sunningdale
 Title: Right Flank Elevation

15/01199/FULL

Status: PLANNING
 Scale: 1 : 100 Date: Oct 2014
 Revision: D Sheet: A3
 Drg No: FSW 14 - 1220-113
 Planning
The absence of a stamp for PLANNING PURPOSES UNIT means not to be used for construction.

WINDSOR RURAL DEVELOPMENT CONTROL PANEL

1 June 2016

Item: 2

Application No.:	16/00645/FULL
Location:	Junction of A30 London Road B383 Broomhall Lane And B383 Chobham Road London Road Sunninghill Ascot
Proposal:	Replace planning permission 09/01219/FULL - Upgrading of existing priority junction to a staggered junction in order to extend the time limit for implementation
Applicant:	Mr McArthur - GCNU Ltd And Crest
Agent:	Mr N J Surtees - Barton Willmore LLP
Parish/Ward:	Sunningdale Parish
If you have a question about this report, please contact: Alistair De Joux on 01628 685729 or at alistair.dejoux@rbwm.gov.uk	

1. SUMMARY

- 1.1 The proposal is to upgrade the existing priority junction of Broomhall Lane and Chobham Road with the A30 London Road by linking the two junctions into a staggered signalised junction. Protected right turn movements for vehicles from the A30 London Road would be incorporated into the layout and signalised pedestrian facilities are also intended at both junctions.
- 1.2 The junction improvements were subject to previous planning permission, the first of which was approved in 2006 and the second in 2009. The more recent permission remains extant as it was granted with a commencement period of seven years. The traffic junction improvements are also required in the implementation of a strategic Runnymede Borough Council planning permission for the former DERA site at Longcross North in Chertsey, which requires the junction improvements to be implemented before the Class B1 development at that site can be occupied.
- 1.3 The Highways officer advises that the lights as previously improved remain as a technically feasible solution to controlling traffic throughout this busy pair of intersections, including both local traffic and additional traffic that will be generated by the redevelopment of the former DERA site at Longcross North.
- 1.4 However, other factors related to the Neighbourhood Plan are noted in this report, such that the design of the lights may not be the most appropriate solution for this part of Sunningdale. In particular, Neighbourhood Plan policy NP/SS5 provides for an area project, the Sunningdale Broomhall Centre, on the north side of the A30 immediately opposite the Chobham Road intersection and also adjacent to the Broomhill intersection in this application. For that reason, the recommendation below provides an opportunity for further work with the developer at the DERA site,

It is recommended the Panel authorises the Borough Planning Manager:	
1.	To grant planning permission on the satisfactory completion of an undertaking to provide that the permission shall not be implemented until: A) the applicant has carried out further studies as to the optimum means of providing appropriate traffic management alongside environmental improvements in support of Neighbourhood Plan policy NP/SS5; B) a Memorandum of Understanding shall be completed to provide a framework for the discussions in regard to (i) above; and with the conditions listed in Section 10 of this report.
2.	To refuse planning permission if an undertaking for the above has not been completed by 30 June 2016, or if satisfactory progress has not been made towards its completion in accordance with an agreed timetable by that date, for the reason that the proposed development has not demonstrated that it is the most appropriate way to give effect to Neighbourhood Plan policies NP/SS5, NP/DG1, NP/DG2 and NP/DG3, and to advice on good design in the NPPF.

2. REASON FOR PANEL DETERMINATION

- The Borough Planning Manager considers it appropriate that the decision on this application be made by the Panel.

3. DESCRIPTION OF THE SITE AND ITS SURROUNDINGS

- 3.1 The application site is the stretch of the A30 London Road from a line across the highway to the north-east of the Chobham Road junction to an equivalent line south-west of the Broomhill Lane junction, along with the approaches in both Chobham Road and Broomhill Lane to their junctions with the A30. As such it forms the road frontages of a number of both residential and commercial properties.

4. DESCRIPTION OF THE PROPOSAL AND ANY RELEVANT PLANNING HISTORY

- 4.1 It appears that the proposed works fall under Class A Part 9 of The Town and Country Planning (General Permitted Development) (England) Order 2015 and, as such, planning permission is not required (consent would be needed from the Highway Authority to work within the highway). However, planning permission has been sought and granted for the same highways works as now applied for, as noted in the table below. This application is intended to extend the time limit for the extant 2009 permission, although it is a full planning application rather than as an application simply to extend the timeframe of the existing permission. For the time being, the 2009 permission remains extant as it was granted with a condition requiring commencement within a period of seven years. The previous planning permissions are as follows:

Ref.	Description	Decision and Date
05/02775/FULL	Upgrading of existing priority junction to a staggered signal junction.	Permitted, 28.06.2006.
09/01219/FULL	Upgrading of existing priority junction to a staggered junction.	Permitted, 12.08.2009.

- 4.2 The proposal is to upgrade the existing priority junction of Broomhall Lane and Chobham Road with the A30 London Road by linking the two junctions into a staggered signalised junction. Protected right turn movements for vehicles from the A30 London Road would be incorporated into the layout and signalised pedestrian facilities are also intended at both junctions. The proposed improvements are shown on Drawing 905/GA/012 at Appendix B to this report.
- 4.3 The 2009 permission also relates to two planning permissions granted by Runnymede Borough Council for the redevelopment of the former DERA site at Longcross North in Chertsey:
- The first of these was granted in 2011 (Ref. RU.05/0538), and provided for 90,624sqm of B1 use plus additional ancillary uses. Condition 17 of that permission required the works permitted by RBWM to be carried out in conjunction with the reserved matter of transport in the Runnymede permission. This is referred to as “the first Runnymede permission” in the discussion below in Section 6 of this report.
 - More recently, planning permission RU.13/0856 has been approved for a hybrid planning permission for the demolition of existing buildings and redevelopment of the site including mixed uses, accesses, landscaping, infrastructure and utility works. Condition 32 of that permission requires the same traffic improvement works to be carried out; no part of the Class B1 development on the Former DERA site may be occupied until the works in Sunningdale have been implemented. This is referred to as “the second Runnymede permission” in the discussion below.
- 4.4 The applicant has now applied to Runnymede Borough Council to remove condition 32 of RU.13/0856. A consultation from Runnymede BC has been received for this application, which is considered elsewhere on this agenda.

5. MAIN RELEVANT STRATEGIES AND POLICIES RELEVANT TO THE DECISION

5.1 National Planning Policy Framework Sections 2, 4, 7, 8 and *Decision-taking*

Royal Borough Local Plan

5.2 The main strategic planning considerations applying to the site and the associated policies are:

	Within settlement area	Highways/Parking issues
RBWM Local Plan	DG1	T5, P4
Ascot, Sunninghill and Sunningdale Neighbourhood Plan	NP/DG1, NP/DG2, NP/DG3, NP/DG5, NP/SS5	NP/T1, NP/T2

Other Local Strategies or Publications

5.3 Other Strategies or publications relevant to the proposal are:

- RBWM Townscape Assessment - view at:
http://www.rbwm.gov.uk/web_pp_supplementary_planning.htm

6. EXPLANATION OF RECOMMENDATION

6.1 The key issues for consideration are:

- Traffic control issues; and
- Streetscape design and relationship of the proposal to the Neighbourhood Plan Strategic Site policy for the Sunningdale Broomhall Centre (NP/SS5).

Traffic control issues

6.2 As noted at in Section 4 above, the junction improvements applied for were first considered as planning applications in 2005 / 06, and approved in 2006. As also noted at 4.3 above, the extant permission for traffic lights at this pair of intersections relates also to the Runnymede Borough Council planning permission RU.13/0856, which requires these works to be implemented before the Class B1 development on the former DERA site in Chertsey can be occupied. Several representations on this application have been received which consider that the impacts of the DERA redevelopment have not been properly considered in the design of the junction improvements that are again being applied for here. However, the applicant's supporting technical statement sets out a case that traffic generated in the second Runnymede permission would be slightly lower than in the first Runnymede permission (as defined in 4.3 above). The Council's Highways Officer concurs with this view.

6.3 The lights previously approved therefore remain as a technically feasible solution to controlling traffic throughout this busy pair of intersections, including both local traffic and additional traffic that will be generated by the redevelopment of the former DERA site at Longcross North.

6.4 One submission received in favour of the proposals has also favoured other local highways improvements, namely increasing the restriction on the Chobham Road bridge over the London-Reading rail line, and the introduction of a 30 mph limit along the whole of Chobham Road. These proposals do not relate to the application site in this application, and do not therefore fall within the scope of what can be required in this application.

6.5 Highways comments are copied in full at Section 7 below.

Streetscape design and environmental enhancements

6.6 Environmental enhancement and traffic management measures were considered alongside the first planning application for traffic lights at this junction, and at that time were subject to Local

Plan policies SND3 and T5. Policy SND3 was an area policy in the Local Plan were provided specifically for environmental enhancement and traffic management measures in Sunningdale, but is not a saved policy. Consideration of the proposal's ability to deliver environmental enhancements alongside appropriate forms of traffic management is now subject to relevant policies in the Neighbourhood Plan. Local Plan policy DG1 also remains relevant.

- 6.7 The Neighbourhood Plan was adopted in 2014, so provides a new policy context for this application. Neighbourhood Plan policy NP/SS5 is an area project for the Sunningdale Broomhall Centre project on the north side of the A30. Part of the site has road frontage to the A30 including land immediately opposite the Chobham Road junction (please refer to plan at Appendix C). Policy NA/SS5 envisages positive and carefully managed change in this area, setting out that any development proposals for the Sunningdale Broomhall Centre must deliver the following improvements and community amenities:
- Improvements to the access to/from the A30 and to the junction of the A30 with the Chobham Road, that also take into account the likely increase in traffic congestion caused by the DERA development in Surrey.
 - Safe and accessible pedestrian walkways and road crossings to link Sunningdale Broomhall Centre to the A30 and to the Chobham Road
 - A public open space
- 6.8 The policy also sets out intentions to provide smaller retail units and housing and supports new a medical / health facility and possibly other small commercial and other development.
- 6.9 The policy seeks to ensure the future viability, vitality and prosperity of Sunningdale's retail centre, noting that it is currently split in two by the A30, the railway line and the difficult, busy junction of the A30 with the Chobham Road. The DERA site in Surrey is specifically considered in the supporting text for this policy, noting its impact on traffic congestion and parking in Sunningdale, especially around the retail centre along the Chobham Road and its junction with the A30, which is aggravated by the nearby level crossing. The policy also notes that the DERA redevelopment could have either a positive impact on Sunningdale's retail economy through increased footfall, or a negative one if traffic congestion discourages existing customers, or a combination of the two.
- 6.10 The design guidance policies in the Neighbourhood Plan would also need to inform the development of this project: Neighbourhood Plan Policy NP/DG1 requires development to respond positively to local townscapes, making particular reference to the Council's Townscape Assessment (TA). While not adopted by the Council as a supplementary planning document, the TA has added significance within the area covered by the Neighbourhood Plan by reason of the specific provisions of policy NP/DG1 which require that development proposals should be compatible with the townscape character area within which they are to be provided. Neighbourhood Plan Policy NP/DG2 requires new development to be similar in density, footprint, separation, scale and bulk of the buildings to that of the surrounding area generally and of neighbouring properties in particular, unless it can be demonstrated that the proposed development would not harm local character. Policy NP/DG3 requires new development to demonstrate good quality design and, as with the above Neighbourhood Plan policies, to respect the character and appearance of the surrounding area. This policy goes on to say that development that fails to take the opportunities available for enhancing the local character and quality of the area and the way it functions should not be permitted; a central part of achieving good design is responding to and integrating with the local surroundings, landscape context and built environment.
- 6.11 In connection with the townscape concerns of The policy NP/DG1, the Townscape Assessment (TA) maps most of the application site within a "Victorian Village" townscape, with the north-western side of the A30 classified as a "Late 20th century suburb" character area. For the Late 20th century suburb an area on the north-western side of the application site, a key characteristic of this townscape type is the unfenced front gardens, wide grass verges and shared amenity greenspaces, which blur the boundary between the public and private realms; however, this is subservient in this area to the Victorian village characteristics of the application site. The TA notes that the experience of the Victorian Village townscape includes main village streets dominated by active frontages that contribute to a sense of vitality, and that the permeability of

the townscape and the human scale of streets ensures a comfortable space and a stimulating environment. The “Guidance / Opportunities” section for this townscape type notes that design should take account of the primary views along the main routes and secondary views up side streets, with occasional views down narrow alleyways and between buildings. Further advice is to retain active street frontages and to co-ordinate the design approach to street furniture, paving and lighting.

- 6.12 The traffic lights proposal was designed well over a decade ago and, while this is a technically workable solution, it may well be the case that the scheme may not provide the ideal layout in the future implementation of Neighbourhood Plan policy NP/SS5. As a scheme design primarily for the management of significant flows of traffic, it is inevitable that it will not be fully sympathetic with the aspirations of both this area policy or with the design guidance policies noted above, NP/DG1, DG2 and DG3. Considered alongside the aspirations of the Broomhall Centre project, it is clear that the traffic lights proposal should not pre-empt the evolving urban form in this important part of Sunningdale. A number of submissions have noted that the future development of the nearby land in Surrey includes the Longcross South site, and that future traffic management in Sunningdale need to take this into account as well. While there is no technical traffic-related reason for resisting the approval of the traffic management system as applied for, it is considered that a mechanism needs to be provided for further investigation into what the optimal scheme should be in for the Sunningdale village centre which will meet the longer term traffic management requirements of this area. For that reason, the applicant has been asked to enter into an agreement to the effect that, if planning permission is granted, no works shall commence on the junction improvements until a mechanism has been agreed to provide for investigation of an alternative form or forms of traffic management that would be more sympathetic to the evolving form of this part of Sunningdale than the current scheme which is oriented primarily to the needs of traffic management.

Other Material Considerations

- 6.13 It appears that the proposed works fall under Class A Part 9 of The Town and Country Planning (General Permitted Development) (England) Order 2015. However, as planning permission has been sought and granted for the same highways works as now applied for, assessment of the application has been carried out on the basis that the applicant has made a formal planning application which the Local Planning Authority is entitled to determine.
- 6.14 The loss of parking to enable the works to the junction is not considered, by itself, to be a reasonable basis on which to refuse planning permission. Car parking is available in other locations within the Broomhall Centre although it is recognised that it may not be as conveniently located for quick trips to a particular shop. The Panel should have regard to the materiality of the previous, and extant, planning consent for the site which had a similar impact on parking provision.

7. CONSULTATIONS CARRIED OUT

Comments from interested parties

176 occupiers were notified directly of the application.

The planning officer posted a statutory notice advertising the application at the site on 22 March 2016.

One letter was received supporting the application, summarised as:

	Comment	Where in the report this is considered
1.	The junction improvement is long overdue	6.3, 6.2.
2.	The junction improvement should be carried out in conjunction with other traffic improvement measures, namely that a 30 mph speed limit should be imposed over the whole of Chobham Road as far as the boundary with Surrey, and a further weight restriction should be	6.4.

	imposed on the Chobham Road bridge over the Reading – Waterloo railway line.	
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Three letters were received objecting to the application, including one from an individual and one each from the Society for the Protection of Ascot and Environs and the Neighbourhood Plan Delivery Group. These are summarised below:

Comment		Where in the report this is considered
1.	There has been a significant change to the A30/ Chobham Road junction and also to the traffic situation since the previous permissions were granted, making the operational capacity assessment of traffic growth from 2009 to 2017 which was relied on as evidence for the extant permission, now irrelevant as it is completely out of date. The scale of development at DERA is now known to be significantly larger with far greater traffic implications.	6.2, 6.3 and Highway Officers comments below.
2.	There has been much development along the A30 leading to an increase in traffic movements.	as above.
3.	Sunningdale Station is a popular station used by many throughout the surrounding greater area thereby increasing the amount of traffic. The planned greater capacity of extra carriages on trains will have an impact on the level crossing closure.	as above.
4.	Frequently traffic backs up way beyond the A30/Chobham Road junction and similarly on the western side of the level crossing. It does not therefore seem unreasonable to ask for an independent assessment of traffic patterns and volumes along the A30, Broomhall Lane, the Chobham Road, without doubt a complex set of intersections. The users are vehicles, trains, cyclists and pedestrians all wanting their little bit of space.	as above.
5.	For the vitality of the heart of our village of Sunningdale so much depends on the viability of the businesses and retailers. The loss of several parking spaces in this scheme would threaten this viability.	as above.
6.	There is no supporting evidence in the applicant's letter that this scheme addresses the requirements of the Neighbourhood Plan Policy NP/ SS5.2 Sunningdale Broomhall Centre strategic site. This scheme will introduce a vast amount of street clutter and urbanise Sunningdale and is contrary to NPPF.	as above.
7.	Runnymede Council have made it a condition of their planning permission that this junction be upgraded before the B1 element of the DERA development is occupied, and this puts pressure on the developer to get this resolved. However it is no justification for asking RBWM to give permission for a scheme that has no evidence to support its viability.	as above.
8.	This is such a major issue and any assessments that are requested should not be confined to the impact of DERA traffic only. For the vitality of the heart of our village of Sunningdale so much depends on the viability of the businesses and retailers. The loss of several parking spaces in this scheme would threaten this viability.	as above.
9.	Runnymede Council have made it a condition of their planning permission that this junction be upgraded before the B1 element of the DERA development is occupied, and this puts pressure on the developer to get this resolved. However it is no justification for asking RBWM to give permission for a scheme that has no evidence to support its viability. This is such a major issue and any assessments that are requested should not be confined to the impact of DERA traffic only.	as above.

Consultees

Consultee	Comment	Where in the report this is considered
Parish Council:	<p>The proposals being put forward under 16/00645 are (almost) identical to 09/01219 which was granted permission on 12th August 2009. However, there have been significant developments since 09/01219 was granted permission both in planning policy and in the traffic developments in this area, which fundamentally change the circumstances. The Parish Council therefore does not regard this as a renewal of extant permission but objects to the application as contrary to Planning Policy and failing to meet the requirements to show the viability of the scheme and its impact on the residents of Sunningdale.</p> <p>Runnymede Council have made it a condition of their planning permission that this junction is upgraded before the B1 element of the DERA redevelopment is occupied and the 2009 renewal is about to expire. Clearly this puts pressure on the developer to resolve this matter, but the worse outcome would be a traffic scheme that does not alleviate traffic problems and changes the nature of the village.</p> <p>Sunningdale Parish Council strongly object to this application for the following reasons:</p> <p><u>1. Viability of the proposed traffic scheme</u></p> <p>There is no evidence presented by the applicant that the solution proposed in this application is the right one to resolve the traffic issues of these 2 junctions. The road traffic capacity assessments which were done when the application was renewed in 2009 predicted road traffic up to 2017 only. In the intervening period traffic volumes have grown significantly, and there have been changes along the Chobham Road such as the building out of the curb at the A30 junction which narrows the road, a restriction of weight on the railway bridge, the introduction of residential parking along the road and permission granted for a pedestrian crossing over the Chobham Road.</p> <p>The scheme would involve the loss of several parking spaces in an area where parking is already a major issue and loss of parking would have a serious detrimental impact on the retailers along the Chobham Road, threatening their viability.</p> <p>An independent traffic survey is essential taking all these matters into consideration, including the likely impact on the traffic on the level crossing – point 2 below, and on pedestrian movements.</p> <p><u>2. Proximity to Sunningdale Railway Level Crossing</u></p> <p>The supporting documentation submitted under 16/00645 by Barton Willmore (19th February 2016, Ref 25773/A3/NS/MO/ef) makes no mention of Sunningdale Railway Level Crossing and no consultation document can be seen from Network Rail or South West Trains for this or</p>	Section 6.

the previous application.

Yet, the Broomhall junction is only some 100 metres away from the Level Crossing, with the high probability of traffic backing up at the proposed new traffic intersections.

Network Rail classifies all level crossings with a 'collective risk number' calculating the risk for all people using the crossing and classifying every level crossing in the UK from 1 (Highest risk) to 13 (least risk). Sunningdale Level crossing is classified at number 3 'Very High Risk'. It is a high risk and high volume crossing. The most recent published Network Rail census lists 100 trains a day, 13,991 Vehicles and 816 Pedestrians or Cyclists a day at this level crossing.

At peak times when the railway level crossing barriers are down there are very significant traffic queues on the A30 from the Bagshot direction. When the railway crossing barriers are raised vehicles will, under the proposed scheme, then have to negotiate a further two traffic lights within 200 metres. Resultant tail backs from both A30 intersections towards the level crossing are inevitable. This, therefore, is likely to increase the risk of an accident at an already high-risk level crossing. Network Rail and South West Trains must be consulted as their approval is essential to the viability of this scheme.

3. National Planning Policy Framework

Since the 2009 application, the NPPF has come into force in March 2012 and it is our contention that this scheme is in violation of Condition 32 and 35. Condition 32 states that all developments that generate significant amounts of movement should be supported by a Transport Statement or Transport Assessment. Plans and decisions should take account of whether:

- the opportunities for sustainable transport modes have been taken up depending on the nature and location of the site, to reduce the need for major transport infrastructure;
- safe and suitable access to the site can be achieved for all people; and
- improvements can be undertaken within the transport network that cost effectively limit the significant impacts of the development. Development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe.

It is the contention of the Parish Council that the cumulative impacts of the development are severe as the scheme threatens the viability of shops, parking and residential amenity of the village. Furthermore, the scheme fails to comply with Condition 35 on almost every point. It will create a significant increase in street clutter (24 traffic signals); it reduces the size of pavement, makes access to RBWM shopper's car park difficult, does not prioritise pedestrians and takes no account of cyclists.

Condition 35 of the NPPF states that Plans (for transport)

	<p>should be located and designed where practical to</p> <ul style="list-style-type: none"> <input type="checkbox"/> give priority to pedestrian and cycle movements, and have access to high quality public transport facilities; <input type="checkbox"/> create safe and secure layouts which minimise conflicts between traffic and cyclists or pedestrians, avoiding street clutter and where appropriate establishing home zones; <input type="checkbox"/> Neighbourhood Plan and Change of Village Setting to Urbanised area; <p>In the Neighbourhood Plan (NP), adopted in April 2014, Sunningdale is classified as a 'Leafy Residential Suburb'. The scheme is contrary to Policy DG1.3 of the NP which states that development proposals in such areas 'should enhance the sylvan, leafy nature of the area, where possible and appropriate , this should include the planting of trees and/ or shrubs along the street.</p> <p>The pedestrian activated traffic lights on the A30 zebra crossing between Broomhall Lane and the Chobham Road will be replaced with a total of 24 timed vehicle traffic lights (14 at Broomhall, Lane and 10 at Chobham Road) within 120 metres of each other. Features designed to enhance the character of Sunningdale such as the Jubilee clock, the Jubilee bench, a number of raised flower beds and grass features will need to be removed. The A30 / Chobham Road intersection is the centre of Sunningdale and the focal point of the Christmas festivities. The current small junctions will be changed into urban intersections and Sunningdale's leafy residential suburb designation will change into that of an urbanised settlement.</p> <p>Furthermore, the scheme is contrary to NP Policy DG3.1 in that the increased traffic volumes anticipated conflict with the pedestrian access to the retail outlets and cafes in Sunningdale.</p> <p>The Parish Council strongly objects to the scheme and recommends:</p> <ul style="list-style-type: none"> <input type="checkbox"/> An independent traffic survey and viability study be conducted; <input type="checkbox"/> Network Rail must be consulted and their comments reports to the public; <input type="checkbox"/> Alternative proposal should be created – not only the developer originated scheme; <input type="checkbox"/> The residents be invited to Public Consultation with the Developers; <input type="checkbox"/> The Borough upholds the NPPF and NP policies, and adopts a resolute position on this in its dealings with the developer; 	
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Other consultees and organisations

Consultee	Comment	Where in the report this is considered

<p>Highways Officer:</p>	<p>The application seeks permission to extend the time limit of the extant permission for the upgrade of the existing priority junction to a staggered signal junction at the junction of the A30 London Road /B383 Broomhall Road, and the B383 Chobham Road. As stated in the submission, fully signalised pedestrian facilities are incorporated into both junctions.</p> <p>With reference to the previous submissions, no changes are proposed to the A30 junction improvement works compared with those submitted in relation to the extant permission.</p> <p>A number of concerns and objections have been raised about the proposal, the majority of which are listed below:-</p> <ul style="list-style-type: none"> <input type="checkbox"/> Traffic on this vicinity of the A30 has increased significantly and this is not reflected in the submission; <input type="checkbox"/> Traffic congestion in the area has had a severe impact on pedestrian movements in the area and vehicular traffic trying to negotiate the junctions; <input type="checkbox"/> The scheme would result in the loss of parking on Chobham Road; <input type="checkbox"/> Proximity of Sunningdale Railway level crossing; <input type="checkbox"/> The application is contrary to National Planning Policy; <p>To address the concerns regarding the increasing level of traffic in the area, I should firstly explain the purpose of a signalised control junction. Where the level of traffic exceeds the operational capacity of a junction, the two main mitigation measures is either to introduce a roundabout or signalise the junction.</p> <p>In this and the previous submissions the applicant has elected to signalise the two junctions. At a signalise junction vehicular traffic is permitted to flow in a strictly controlled manner to ensure that the side roads are given the opportunity to cross or join the main road. The side roads referred to in this instance are Chobham Road and Broomhall Road.</p> <p>As stated in the application dedicated signal phases for pedestrians are included, providing substantial safety benefits for pedestrian movement and activity in the immediate area.</p> <p>Concerns have also been expressed about the loss of car parking spaces, particularly on Chobham Road. The plan accompanying the submission [Proposed A30 Signal Improvements Broomhall Road and Chobham Road, Sunningdale – 905/GA/012 Rev B] show that an 8m of the on street parking spaces will be relocated on Chobham Road.</p> <p>The Sunningdale Railway Level crossing is situated circa 100 metres west of Broomhall Road junction. During the peak periods when the barrier is down, there is significant queuing on both sides of the barriers. The Borough’s Highway and Traffic Engineer has commented that these traffic queues can be controlled more effectively with traffic</p>	<p>Section 6.</p>
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	<p>signals. These controls could be synchronised with the barriers to include a 'train' phase within the cycle. The developer would need to consult with Network Rail to gain their approval to include this phase into the signals.</p> <p>Reference is being made to paragraph 32 of the National Planning Policy Framework (NPPF) which states that,</p> <p>All developments that generate significant amounts of movement should be supported by a Transport Statement or Transport Assessment. Plans and decisions should take account of whether:</p> <ul style="list-style-type: none"> <input type="checkbox"/> the opportunities for sustainable transport modes have been taken up depending on the nature and location of the site, to reduce the need for major transport infrastructure; <input type="checkbox"/> safe and suitable access to the site can be achieved for all people; <input type="checkbox"/> and improvements can be undertaken within the transport network that cost effectively limit the significant impacts of the development. Development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe; <p>The application proposes improving traffic flows across the 2 priority junctions. In Highways terms this is unlike a B1 or A3 use. Therefore, the application is not considered to be a traffic generator. However, within Promoting Sustainable Transport, paragraph 31 of the NPPF does state that,</p> <p>Local authorities should work with neighbouring authorities and transport providers to develop strategies for the provision of viable infrastructure necessary to support sustainable development.</p> <p>Nonetheless, if the Planning Authority is minded to refuse the application, this would need to be supported by robust evidence clearly demonstrating that signalising the junctions would have an adverse effect upon the safe and free movement of traffic, and would lead to further increases in traffic congestion in the surrounding area.</p> <p>However, based on the submission the Highway Authority recommends approval subject to the inclusion of the following condition:</p> <p>Grampian condition linking this application to the development proposal requiring the applicant to enter into an agreement under Section 278 of the Highways Act 1980 to secure the implementation of this scheme.</p>	
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8. APPENDICES TO THIS REPORT

- Appendix A - Site location plan
- Appendix B - layout drawing
- Appendix C - Broomhall Centre location map for Policy NP/SS3


This recommendation is made following careful consideration of all the issues raised through the application process and thorough discussion with the applicants. The Case Officer has sought solutions to these issues where possible to secure a development that improves the economic, social and environmental conditions of the area, in accordance with NPFF.
In this case the issues have / have not been successfully resolved.

9. CONDITIONS RECOMMENDED FOR INCLUSION IF PERMISSION IS GRANTED

1. The development hereby permitted shall be commenced within three years from the date of this permission.
Reason: To accord with the provisions of Section 91 of the Town and Country Planning Act 1990 (as amended).

2. The development hereby permitted shall be carried out in accordance with the approved plans listed below.
Reason: To ensure that the development is carried out in accordance with the approved particulars and plans.


DO NOT SCALE



STAGE ALLOCATION	1	2	3	4
Diagram 1				

RECEIVED	16 JUL 2008				
PLANNING	ACROSS TOLLWAY CORNER SIDE OF A14				
REV	DATE	BY	DESCRIPTION	CHK	APP

DRAFT



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CLIENT: CREST NICHOLSON DEVELOPMENT LTD + HOLLEY LAND MANAGEMENT

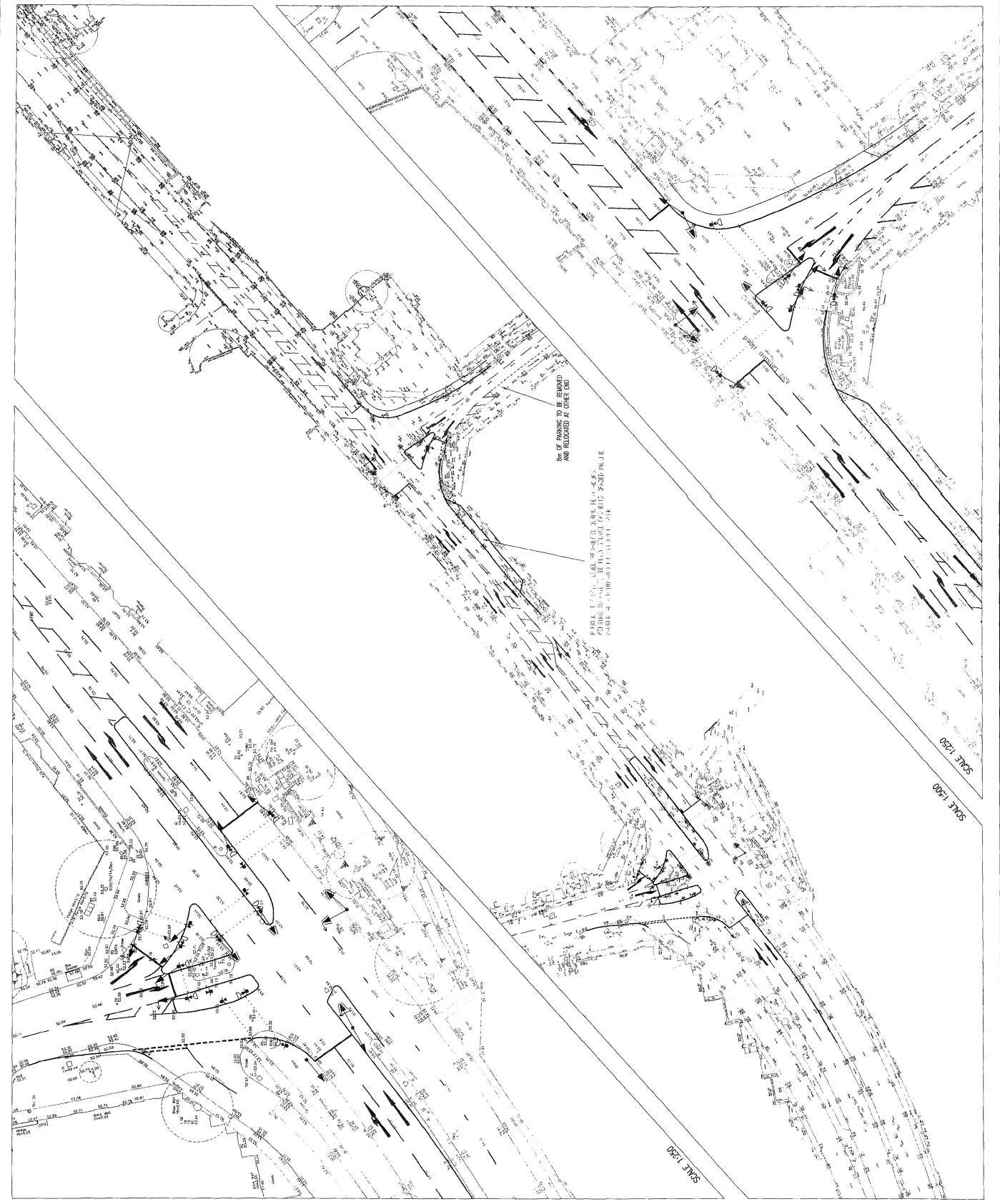
PROJECT: SCOTT BROWNRIGGS

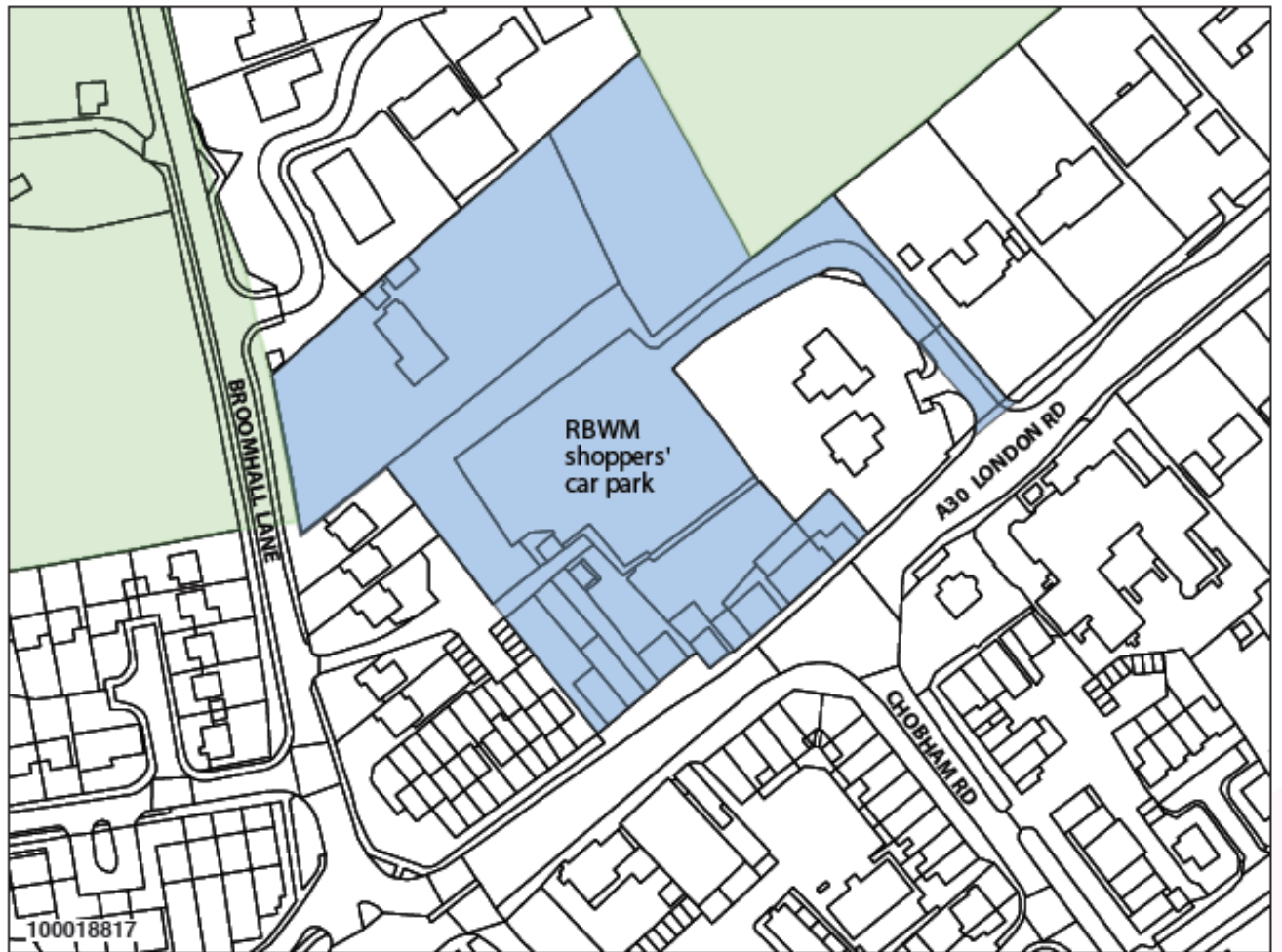
PROJECT NO.: 0/0219

TITLE: PROPOSED A30 SIGNAL IMPROVEMENTS
 BROOMHALL ROAD AND CHOBHAM ROAD,
 SUNNINGDALE

ISSUED BY:	AS S+J/VA	APPROVED BY:	SVY
DRAWN BY:	INGRAM/ST	CHECKED BY:	DTL
DATE:	11/07/08	DATE:	JULY 2008
PROJECT NO.:	11010905	DRAWING NO.:	908GA012
REV:		REV:	B

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WINDSOR RURAL DEVELOPMENT CONTROL PANEL

1 June 2016

Item: 3

Application No.:	16/30007/SMI
Location:	Former DERA Site Chobham Lane Longcross Chertsey
Proposal:	Removal of condition 32 (requirement to improve junction at A30 London Road/Broomhall Lane/Chobham Road) of planning permission RU.13/0856 (Hybrid planning permission for the demolition of existing buildings and redevelopment of the site including mixed uses, accesses, landscaping, infrastructure and utility works.
Applicant:	Christine Kelso
Agent:	Not Applicable
Parish/Ward:	Sunninghill And Ascot Parish

If you have a question about this report, please contact: Alistair De Joux on 01628 685729 or at alistair.dejoux@rbwm.gov.uk

1. SUMMARY

- 1.1 This report deals with a consultation received by the Royal Borough from Runnymede Borough Council (RBC) on an application to that local planning authority to remove condition 32 of permission reference RU.13/0856.
- 1.2 Runnymede planning permission RU.13/0856 is hybrid permission for the redevelopment of the former DERA site at Longcross North in Chertsey. It includes a detailed first phase of 108 dwellings and outline permission for up to 79,025 sq.m. Class B1 employment uses (including parking); up to 36,000 sq .m. of *sui generis* data centres use (including ancillary facilities and parking) and up to another 92 dwellings (200 in total including the detailed phase already permitted). A range of associated facilities including internal roads, public open space, ecological habitats and general amenity areas.
- 1.3 Condition 32 requires that no part of the B1 floorspace may be occupied until the works permitted under RBWM permission 09/01219/FULL have been completed.
- 1.4 This consultation request from RBC is being considered alongside an application for the same works permitted under RBWM permission ref. 09/01219/FULL.

It is recommended the Panel requests that Runnymede Borough Council take the following matters into consideration when determining this application:

The Royal Borough of Windsor and Maidenhead objects to the proposal to remove condition 32 and requests that the condition is varied for the reasons set out below:

- (i) Planning permission RU.13/0856 was approved subject to off-site highways works being provided, which were intended to mitigate traffic generated by both the construction phase and the completed development. While RBWM has previously consented traffic light controlled junctions at Broomhall Lane/A30 and A30/Chobham Road under consent 09/01219/FULL the Council would request the condition to now be varied to require the applicant to submit a scheme for works to those junctions to be approved by the Local Planning Authority [which would be RBC]. This variation would allow for the ongoing discussions set out at (ii) and (iii) involving local residents and have regard to the adopted Development Plan.
- (ii) The Royal Borough notes that the traffic management plan previously approved under RBWM permission ref. 09/01219/FULL was designed over ten years ago, and while it would be technically feasible to implement the permission this has not been done until now due to commercial decisions to delay the development. Since that permission was issued, the local planning framework has changed with the adoption of the Ascot, Sunninghill and Sunningdale Neighbourhood Plan 2011 - 2026. Neighbourhood Plan policy NP/SS5 is an area project for the Sunningdale Broomhall Centre project on the north side of the A30, where part of the site has road frontage to the A30 including land immediately opposite the Chobham Road junction. While traffic management

measures will be required in the near future, it is considered that the measures need to be considered as part of the design process for the Sunningdale Broomhall Centre site.

- (iii) In the interests of allowing for effective cross-boundary working, the applicants have been asked to enter into discussion with RBWM to carry out further studies on the most appropriate form of traffic management plan that will be sympathetic to the aspirations of the local community and assist in giving effect to the area project in Neighbourhood Plan policy NP/SS5.

2. REASON FOR PANEL DETERMINATION

- The Borough Planning Manager considers it appropriate that the Panel determines the application.

3. DESCRIPTION OF THE SITE AND ITS SURROUNDINGS

- 3.1 The application site is part of the former “DERA North” site which is located to the north of the M3 motorway, on the western edge of the Borough of Runnymede (RBC in this report), adjacent to its boundary with Surrey Heath Borough. The site is broadly triangular in shape and is bounded by the Reading to Waterloo railway line to the northern boundary and Chobham Lane and the M3 to the southern boundary. The area of the application site is 33.6 hectares, all of which is located within Runnymede Borough. The DERA North site also includes 7.7 hectares of land which is located within Surrey Heath Borough and is bounded to the west by Burma Road.
- 3.2 The RBWM interest in this development relates to traffic impacts on Sunningdale and in particular on the Sunningdale Village Centre, which condition 32 of the RBC consent seeks to control.

4. DESCRIPTION OF THE PROPOSAL AND ANY RELEVANT PLANNING HISTORY

- 4.1 The site has a planning history dating back to 2005 and includes several applications in Runnymede Borough and some within Surrey Heath. Consultations for these were received from both Councils. The planning permission that the current application relates to is a hybrid permission that includes both outline and detailed components, as follows:

Hybrid planning application for the demolition of existing buildings and redevelopment of the site to provide; up to 79,025sqm (GEA) of Class B1 employment uses (including parking); up to 36,000sqm (GEA) of sui generis Data Centres use (including ancillary facilities and parking); up to 200 dwellings, including a detailed first phase comprising 108 dwellings (comprising 13 x two bed, 26 x three bed, 21 x four bed and 13 x five bed dwellings; 8 x one bed apartments and 23 x two bed apartments; and 2 x one bed FOGs and 2 x two bed FOGs); roadways driveways and pavements; fencing and walling; up to 6,300sqm (GEA) of ancillary uses, including Class A1 - A5 uses (i.e. retail uses, cafe/restaurants and a public house up to 1,550sqm GEA), Class D1 uses (i.e. childcare facilities up to 600sqm GEA); Class D2 uses (i.e. Health and Leisure (up to 1900sqm GEA); the creation of Publicly Accessible Open Space (PAOS), ecological habitats, general amenity areas (including informal and formal open spaces), equipped play areas and landscaped areas; new vehicular accesses from the existing public highway network; vehicle and cycle parking; bin stores; landscape compound; car parking (for railway station); electricity sub-stations; lighting; drainage and associated infrastructure works, including sustainable drainage systems (SUDS); a foul pumping station; an acoustic fence and associated engineering and service operations.

- 4.2 RBWM was consulted on this application in August 2013 (RBWM reference 13/30019/SMI), and again in December 2013 on an Addendum to the Environmental Statement provided by the applicants in accordance with Regulation 22 of the Environmental Impact Regulations (RBWM 13/30030/SMI). This application included full detailed matters for Phase 1, which is for the construction of 108 dwellings, along with outline details of the redevelopment of the remainder of the site. Both of these consultations were responded to with letters setting out the following requests and concerns:

- prior to the determination of the application an updated Transport Assessment should be submitted, which should include revised traffic count modelling. The Council requested that it be consulted on the Transport Assessment.
- Junction improvements to the A30 London Road / Broomhall Lane/ Chobham Road junction should take place in accordance with RBWM planning permission 09/01219/FULL, in accordance with condition 17 of a previous permission granted by Runnymede Borough Council.
- Commuter parking at Longcross Station should be considered, to relieve existing pressure on the local highway network.
- A demolition, construction and implementation traffic management plan be secured by condition, and in order to ensure the free flow of traffic and manage the implications of the traffic on the road network within the Borough of Windsor and Maidenhead that routing is discussed with RBWM. It was also requested that construction / demolition traffic do not use Chobham Road to link to the A30.
- It was requested that a condition be imposed to require all demolition waste should be recycled on site and to secure this.

4.3 Runnymede Borough Council again consulted RBWM on details submitted for the same application (RU.13/0856). RBWM responded with a letter of objection dated 5 February 2014, noting that the applicant's highways consultants advice was inconsistent in regard to highways capacity at the A30 London Road / Chobham Road / Broomhall Lane junction and that the highways improvements at this junction should be in place before any development commences on the DERA site because of existing capacity issues.

4.4 Following this, Runnymede BC approved application reference RU.13/0856 on 14 August 2015. The Royal Borough has since then been consulted on the details submitted in respect to the Construction Environmental Management Plan required by Condition 18 for Phase 1 (the detailed component of the application for 108 houses).

4.5 Condition 32 of the Runnymede permission, which the current application to that Council now seeks to remove, is:

No part of the Class B1 use development hereby approved shall be occupied until the improvements to the A30 London Road/Broomhall Lane/Chobham Road junction (including the signalisation of that junction) as outlined on drawing number 905/GA/012 Rev B (Appendix H of the submitted Transport Assessment) submitted with the application and in accordance with planning permission number 09/01219 issued by the Royal Borough of Windsor and Maidenhead have been completed.

Reason: in order that the development should not prejudice highway safety nor cause inconvenience to other road users and to comply with saved policies MV3 and MV4 of the Runnymede Borough Local Plan Second Alteration 2001 and guidance in the NPPF.

4.6 The following applications within RBWM relate to works at the Junction Of A30 London Road B383 Broomhall Lane And B383 Chobham Road, Sunningdale:

05/02775/FULL	Upgrading of existing priority junction to a staggered signal junction.	28.06.2006, with a five-year condition for the commencement of the development.
09/01219/FULL	Upgrading of existing priority junction to a staggered junction.	Permitted 12.08.2009, with a seven-year condition for the commencement of the development.

5. MAIN RELEVANT STRATEGIES AND POLICIES RELEVANT TO THE DECISION

5.1 National Planning Policy Framework Sections 2, 4, 7, 8 and *Decision-taking*

Royal Borough Local Plan

5.2 The main strategic planning considerations applying to the site and the associated policies are:

	Within settlement area	Highways/Parking issues
RBWM Local Plan	DG1	T5, P4
Ascot, Sunninghill and Sunningdale Neighbourhood Plan	NP/DG1, NP/DG2, NP/DG3, NP/DG5, NP/SS5	NP/T1, NP/T2

Other Local Strategies or Publications

5.3 Other Strategies or publications relevant to the proposal are:

- RBWM Townscape Assessment - view at:
http://www.rbwm.gov.uk/web_pp_supplementary_planning.htm

6. EXPLANATION OF RECOMMENDATION

6.1 The key issues are:

- Whether the removal of condition 32 would result in unacceptable impacts on traffic in Sunningdale.
- Whether a different traffic management proposal would result in a more acceptable solution for residents.

Impacts on traffic in Sunningdale

- 6.2 As noted at in Section 4 above, the junction improvements required by condition 32 were previously approved in 2006 under RBWM ref. 05/02775/FULL. The extant permission ref. 09/01219/FULL is linked to Runnymede Borough Council planning permission RU.13/0856, which requires these works to be approved by RBWM before the Class B1 development on the former DERA site in Chertsey to be occupied before the traffic improvement works in Sunningdale have been carried out. Several representations on this application have been received which consider that the impacts of the DERA redevelopment have not been properly considered in the design of the junction improvements. However, the applicant's supporting technical statement sets out a case that traffic generated in the second Runnymede permission would be slightly lower than in the first Runnymede permission (as defined in 4.3 above). The Council's Highways Officer concurs with this view.
- 6.3 The lights previously approved therefore remain as a technically feasible solution to controlling traffic throughout this busy pair of intersections, including both local traffic and additional traffic that will be generated by the redevelopment of the former DERA site at Longcross North.
- 6.4 Future impacts from the Longcross redevelopment will not be limited to the impacts resulting from the existing permission however; the future development of the Longcross South site may also result in further future traffic impacts in Sunningdale, and it appears to be highly likely that this may also need to be provided for in future traffic improvements in the village.

Streetscape design and environmental enhancements

- 6.5 Environmental enhancement and traffic management measures were considered alongside the first planning application for traffic lights at this junction, and at that time were subject to Local Plan policies SND3 and T5. Policy SND3 was an area policy in the Local Plan that provided specifically for environmental enhancement and traffic management measures in Sunningdale, but is not a saved policy. Consideration of the proposal's ability to delivery environmental enhancements alongside appropriate forms of traffic management is now subject to relevant policies in the Neighbourhood Plan. Local Plan policy DG1 also remains relevant.

- 6.6 The Neighbourhood Plan was adopted in 2014, so provides a new policy context for this application. Neighbourhood Plan policy NP/SS5 is an area project for the Sunningdale Broomhall Centre on the north side of the A30. Part of the site has road frontage to the A30 including land immediately opposite the Chobham Road junction (please refer to plan at Appendix C). Policy NA/SS5 envisages positive and carefully managed change in this area, setting out that any development proposals for the Sunningdale Broomhall Centre must deliver the following improvements and community amenities:
- Improvements to the access to/from the A30 and to the junction of the A30 with the Chobham Road, that also take into account the likely increase in traffic congestion caused by the DERA development in Surrey.
 - Safe and accessible pedestrian walkways and road crossings to link Sunningdale Broomhall Centre to the A30 and to the Chobham Road.
 - A public open space.
- 6.7 The policy also sets out intentions to provide smaller retail units and housing and supports new a medical / health facility and possibly some additional small commercial and other development.
- 6.8 The policy seeks to ensure the future viability, vitality and prosperity of Sunningdale's retail centre, noting that it is currently split in two by the A30, the railway line and the difficult, busy junction of the A30 with the Chobham Road. The DERA site in Surrey is specifically considered in the supporting text for this policy, noting its impact on traffic congestion and parking in Sunningdale, especially around the retail centre along the Chobham Road and its junction with the A30, which is aggravated by the nearby level crossing. The policy also notes that the DERA redevelopment could have either a positive impact on Sunningdale's retail economy through increased footfall, or a negative one if traffic congestion discourages existing customers, or a combination of the two.
- 6.9 While it is not considered that condition 32 in the Runnymede permission should be removed, it is important that the road junction in this area should be designed in conjunction with a development brief for this area, as provided for in Policy NP/SS5. The design guidance policies in the Neighbourhood Plan should also inform the development of this project: Neighbourhood Plan Policy NP/DG1 requires development to respond positively to local townscapes, making particular reference to the Council's Townscape Assessment (TA). While not adopted by the Council as a supplementary planning document, the TA has added significance within the area covered by the Neighbourhood Plan by reason of the specific provisions of policy NP/DG1 which require that development proposals should be compatible with the townscape character area within which they are to be provided. Neighbourhood Plan Policy NP/DG2 requires new development to be similar in density, footprint, separation, scale and bulk of the buildings to that of the surrounding area generally and of neighbouring properties in particular, unless it can be demonstrated that the proposed development would not harm local character. Policy NP/DG3 requires new development to demonstrate good quality design and, as with the above Neighbourhood Plan policies, to respect the character and appearance of the surrounding area. This policy goes on to say that development that fails to take the opportunities available for enhancing the local character and quality of the area and the way it functions should not be permitted; a central part of achieving good design is responding to and integrating with the local surroundings, landscape context and built environment.
- 6.10 In connection with the townscape concerns of policy NP/DG1, the Townscape Assessment (TA) maps most of the application site within a "Victorian Village" townscape, with the north-western side of the A30 classified as a "Late 20th century suburb" character area. For the Late 20th century suburb an area on the north-western side of the application site, a key characteristic of this townscape type is the unfenced front gardens, wide grass verges and shared amenity greenspaces, which blur the boundary between the public and private realms; however, this is subservient in this area to the Victorian village characteristics of the application site. The TA notes that the experience of the Victorian Village townscape includes main village streets dominated by active frontages that contribute to a sense of vitality, and that the permeability of the townscape and the human scale of streets ensures a comfortable space and a stimulating environment. The "Guidance / Opportunities" section for this townscape type notes that design should take account of the primary views along the main routes and secondary views up side streets, with occasional views down narrow alleyways and between buildings. Further advice is

to retain active street frontages and to co-ordinate the design approach to street furniture, paving and lighting.

- 6.11 The traffic lights proposal was designed well over a decade ago, and while this is a technically workable solution, it may well be the case that the scheme may not provide the ideal layout in the future implementation of Neighbourhood Plan policy NP/SS5. As a scheme design primarily for the management of significant flows of traffic, it is inevitable that it will not be fully sympathetic with the aspirations of both this area policy or with the design guidance policies noted above, NP/DG1, DG2 and DG3. Considered alongside the aspirations of the Broomhall Centre project, it is clear that the traffic lights proposal should not pre-empt the evolving urban form in this part of Sunningdale.

Other considerations

- 6.12 An issue has been raised locally with apparent discrepancies in traffic counts taken by RBWM on the A30 and as provided by the developer. The developer has been asked for further information and clarification on this point.

Conclusions

- 6.13 Future traffic management in Sunningdale need to take into account the impacts from the approved development at the Longcross North site including those from construction traffic and from future residents and business occupiers, further impacts for the future development of the Longcross South site, and the future redevelopment in Sunningdale in accordance with Neighbourhood Plan policy NP/SS5. For that reason, it is considered that the Royal Borough's representation to RBC should object to the outright removal of condition 32, and recommended that a modified condition 32 provides for highways improvements to be considered that provide a more optimal design solution that takes into account both traffic management and environmental enhancements in Sunningdale.

7. CONSULTATIONS CARRIED OUT

Comments from interested parties

One letter has been received objecting to the application at the time of writing, from Councillor L. Evans writing in her capacity as a Ward Councillor.

Comment		Where in the report this is considered
1.	Apparent discrepancies in traffic counts taken by RBWM on the A30 and as provided by the developer.	6.13.

Consultee comments

Consultee	Comment	Where in the report this is considered
Parish Council:	No comments had been received at the time of writing, and any received will be reported in an update.	To be included in an update report.

Consultee	Comment	Where in the report this is considered
Highways Officer:	No comments had been received at the time of writing, and any received will be reported in an update.	To be included in an update

		report.
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8. APPENDICES TO THIS REPORT

- Appendix A - Site location plan
- Appendix B - Plan of improvements provided for in the extant permission, and as required by condition 32 of RU.13/0856.

9. RECOMMENDATION

9.1 OBJECTION, as noted in Section 1 in this report.

WINDSOR RURAL DEVELOPMENT CONTROL PANEL

1 June 2016

Item: 4

Application No.:	16/00691/FULL
Location:	Earley Edge 2 Fir Tree Close Ascot SL5 9LJ
Proposal:	Erection of 1 x 6 bedroom dwelling with attached garage and associated accommodation over and new front entrance gates following demolition of existing dwelling.
Applicant:	Mr Gunther
Agent:	Mr Warren Joseph
Parish/Ward:	Sunninghill And Ascot Parish
If you have a question about this report, please contact: Adam Jackson on 01628 796660 or at adam.jackson@rbwm.gov.uk	

1. SUMMARY

- 1.1 The proposed dwelling is considered to be of an acceptable design and scale for the surrounding area. Subject to the submission of an acceptable landscaping plan the impact on the street scene is considered to be acceptable and there is sufficient space either side of the dwelling to prevent it from appearing cramped on site.
- 1.2 There is sufficient space either side of the dwelling and sufficient boundary screening to protect the residential amenity of neighbouring properties.
- 1.3 Subject to the submission of a satisfactory tree protection plan the council is satisfied that there would be no negative impact to protected/important on site trees.
- 1.4 Sufficient vehicle parking space has shown to be provided and there would be no adverse effect on highway safety.

It is recommended the Panel grants planning permission subject to the conditions listed in Section 10 of this report.

2. REASON FOR PANEL DETERMINATION

- At the request of Councillor Hilton if the recommendation is to grant the application at the request of the Parish Council and the Neighbourhood Plan Delivery Group.

3. DESCRIPTION OF THE SITE AND ITS SURROUNDINGS

- 3.1 The site is located on the southern side of Fir Tree Close, Ascot and is in an area defined as Villas in a Woodland Setting. The site comprises of an existing residential property. The existing property is a two storey detached dwelling with a single storey attached garage. The garden of the site is covered by an area TPO and is heavily planted to the side (west) and to the rear. Fir Tree Close comprises of a small number of large detached properties set within spacious plots and in general these are finished in light coloured brick and have hipped roofs. The sites are also in general well screened by high levels of planting, and some of the properties have gated access as well.

4. DESCRIPTION OF THE PROPOSAL AND ANY RELEVANT PLANNING HISTORY

- 4.1 There is no relevant planning history for the site; however, there has been a similar approval for a replacement dwelling at number 6 Fir Tree Close which is opposite.
- 4.2 The proposal is for a two storey replacement dwelling with additional accommodation in the roof, together with a double garage. The ridge height of the dwelling is 9.4 metres and the eaves height is 6.7 metres. The height of the garage is 6.7 metres and the eaves height is 3.8 metres. The width of the dwelling is also approximately 20 metres. These are similar dimensions to the

property approved opposite at number 6. The proposed dwelling has a hipped roof and includes 2 flat roof dormers to the rear. The proposed dwelling will be set slightly further back in the plot and it is proposed to add additional planting and a gated access to the front.

5. MAIN RELEVANT STRATEGIES AND POLICIES RELEVANT TO THE DECISION

5.1 National Planning Policy Framework, Section 7 – Requiring good design

Royal Borough Local Plan

5.2 The main strategic planning considerations applying to the site and the associated policies are:

	Within settlement area	Protected Trees	Highways /Parking issues
Local Plan	DG1, H10, H11	N6	T5, P4
Ascot, Sunninghill and Sunningdale Neighbourhood Plan	NP/H2, NP/DG1, NP/DG2, NP/DG3, NP/DG5	NP/EN2, NP/EN3	NP/T1, NP/T2

5.3 Supplementary planning documents adopted by the Council relevant to the proposal are:

- Sustainable Design and Construction
- Planning for an Ageing Population

More information on these documents can be found at:

http://www.rbwm.gov.uk/web/pp_supplementary_planning.htm

Other Local Strategies or Publications

5.4 Other Strategies or publications relevant to the proposal are:

- RBWM Landscape Character Assessment - view at: http://www.rbwm.gov.uk/web_pp_supplementary_planning.htm
- RBWM Townscape Assessment - view at: http://www.rbwm.gov.uk/web_pp_supplementary_planning.htm
- RBWM Parking Strategy - view at: http://www.rbwm.gov.uk/web_pp_supplementary_planning.htm

6. EXPLANATION OF RECOMMENDATION

6.1 The key issues for consideration are:

- i Design and the impact on character
- ii Impact on residential amenity
- iii Impact on important trees
- iv Parking and highway safety

Design and impact on character

6.2 The site is set within an area defined as 'Villas in a Woodland Setting' by the Townscape Assessment. The key characteristics of this area include; large dwellings within large garden plots set and set well back from the road, varied building styles and roofscapes and mature trees

and planting which give the impression of dwellings 'in woodland'. It is considered that the proposed dwelling respects the key characteristics of this townscape area and as such the design of the proposal is considered acceptable and in accordance with policy DG1 of the Neighbourhood Plan. The proposed dwelling is larger than the other properties currently in the street (The proposed dwelling is approximately 1.8 metres taller than number 3 and 0.9 metres taller than number 1), however, the plot is considered large enough to accommodate this size and style of dwelling. Amended plans have been received which set the dwelling a further 1.5 metres off the west boundary and 1 metre off the east boundary and it is considered that there is sufficient space either side of the dwelling to prevent the development appearing cramped. The proposed dwelling is set 2.6 metres (at its closest point) off the boundary shared with No. 3 and 7.5 metres off the boundary with No.01. The proposed dwelling is also set back approximately 11 metres from the front boundary which is more than the existing dwelling.

- 6.3 In addition the form of development on the road is not rigid and the scale of development does vary. It is considered therefore that the design and scale of the dwelling can differ from the other dwellings on the road without negatively impacting on the character and appearance of the street scene. The dwelling would maintain the stagger in built form between number 1, 2 and 3 and the set back from the road would ensure that it does not appear overly dominant in the street scene. This is also helped by the pitched roof proposed for both the main dwelling and the garage which slopes away from the street. The proposal is therefore considered to comply with policy DG2 of the Neighbourhood Plan.
- 6.4 Policy DG3 of the Neighbourhood Plan also requires good design. For the reasons set out above it is considered that the proposed dwelling would respect the character and appearance of the surrounding area. Details of materials can be secured by condition (See condition 2) as can acceptable landscaping and boundary treatment (see condition 5). There is also sufficient space on the site for cycle and refuse storage. The proposed dwelling has a crown roof which would make the dwelling bulkier than other properties in the street; however, given the size of the plot and the set back of the dwelling it is considered that this design is acceptable as the prominence of the dwelling will be reduced. The proposal will also introduce a dormer into the street; however, it is of modest size and is set well within the roof of the garage and it is not considered therefore that the dormer would appear out of keeping for the area. It is also worth noting that the scale and design of the proposed dwelling is very similar to that approved at number 6 Fir Tree Close which is opposite the application site.

Impact on residential amenity

- 6.5 The proposed dwelling has two first floor side windows in the west elevation; however, these serve bathrooms and as such can be obscurely glazed as suggested in condition 7. In addition the protected trees along this boundary provide a high level of screening which will prevent views being afforded into neighbouring gardens. It is also considered that the proposed dwelling is set far enough from both boundaries to prevent it appearing overbearing to neighbouring properties. On the east side is the two storey garage which is at its closest point is approximately 6.4 metres from number 3. Along this eastern boundary there is hedging and planting which reaches heights of approximately 5 metres which provides a high level of screening, this planting is being shown to be retained and can be secured for a period of 5 years (see condition 5). The 25 degree test is complied with and there are front and rear windows in addition to the side windows which also serve number 3's lounge. As such an acceptable level of light would be maintained for this room. There are also no first floor side windows at number 3. On the west of the site side there is a gap of over 10 metres to number 1 Fir Tree Close.

Impact on important trees

- 6.6 There are a number of protected trees within the site, the most important being located along the western boundary. The proposed dwelling would be outside of the root protection areas for all protected trees. Amended plans have also been submitted to move the proposed dwelling a further 1.5m away from the western boundary to prevent trees from being damaged during construction and to remove any pressure for trees to be removed or pruned in the future due to overshadowing of the dwelling. Subject to the inclusion of suitable conditions the impact on trees is considered to be acceptable (see conditions 3, 4 and 5). The existing cypress hedging along

the eastern boundary is also shown to be retained and is secured by a period of 5 years by condition 5).

Parking and highway safety

- 6.7 The carriageway is 5.9 metres wide and the footway is 1.6 metres wide. It is considered necessary for details to be submitted demonstrating that sufficient visibility splays are retained. It is proposed to add gates which are set back 2.5 metres from the carriageway. Ordinarily it is necessary for gates to be set back 5 metres to allow for cars to pull off the highway whilst gates are opened/closed, however, as the road is a cul-de-sac traffic will be low and 2.5 metres is therefore considered acceptable.
- 6.8 A 6/7 bedroom dwelling in this location generates a requirement for 3 car parking spaces. The plans provided show a double garage which meets the borough's current requirements to provide 2 parking spaces. The hardstanding to the front of the site is sufficient to provide the remaining required space as well as a turning area to allow vehicles to enter and exit the site in a forward gear.

7. CONSULTATIONS CARRIED OUT

Comments from interested parties

7 occupiers were notified directly of the application.

The planning officer posted a statutory notice advertising the application at the site on 15.03.2016

5 letters were received objecting to the application, summarised as:

Comment	Officer response
1. The scale and height of the dwelling would be out of keeping with the other properties in fir tree close.	See section 6.2, 6.3 and 6.4.
2. The scale of the development would negatively impact on the street scene.	See section 6.2, 6.3 and 6.4.
3. The proposal represents overdevelopment of the plot.	See section 6.2, 6.3 and 6.4.
4. The proposed dwelling would negatively impact on residential amenity.	See section 6.5.
5. The proposed dwelling would likely result in the loss of trees.	See section 6.6.
6. Lack of planting/landscaping and tree protection information.	See condition 5.
7. The applicant's tree report is inaccurate.	See condition 3.

Other consultees and organisations

Consultee	Comment	Officer response
Neighbourhood Plan Delivery Group	<ul style="list-style-type: none"> i. The plans are misleading. The site plan shows two buildings to be demolished which are actually a marquee/tent and a greenhouse and the site section plan shows a tree between No.2 and No.3 which does not exist. ii. The overall scale, mass and bulk of the proposed building is too large for the size/shape of the plot and its location. The proposed scheme will look cramped. iii. The separation distance between the garage and the boundary with No.3 Fir Tree Close is just 1.6 metres; this will have a serious impact on the 	<ul style="list-style-type: none"> 1. Noted – The site section plan is not included in the list of approved plans. 2. See section 6.2. 3. See section 6.5. 4. See section 6.2, 6.3 and 6.4.

	<p>privacy and amenity of the neighbours at No.3. There is no tree or hedge boundary treatment between the proposed garage block and No.3.</p> <p>iv. The proposed dwelling is out of character with the area.</p> <p>v. Insufficient tree protection information has been submitted.</p> <p>vi. We challenge whether the proposed driveway provides adequate parking for visitors and tradesmen.</p>	<p>5. See section 6.6.</p> <p>6. See section 6.8.</p>
SPAE	The proposed dwelling will have a cramped and over developed appearance. The mass of the proposed dwelling fails to respect the character of Fir Tree Close which enjoys a more spacious appearance with greater distances from site boundaries.	See section 6.2, 6.3 and 6.4.
Parish Council	Objections on the grounds of scale mass and bulk. The committee considered the proposal to be an overdevelopment of the site and unneighbourly to number 3 Fir Tree Close. No landscape plan has been submitted which is material to the determination of the application.	See sections 6.2, 6.3, 6.4 and 6.6 and condition 5.

Consultee	Comment	Officer response
Highway Officer	<p>Recommends approval subject to conditions relating to;</p> <ul style="list-style-type: none"> <input type="checkbox"/> Provision of the parking and vehicle turning space <input type="checkbox"/> Use of the garage for vehicle parking only <input type="checkbox"/> Provision of visibility splays 	See conditions 8, 9 and 10.
Tree Officer	<p>Recommends the following conditions;</p> <ul style="list-style-type: none"> <input type="checkbox"/> Tree protection <input type="checkbox"/> Tree retention/replacement <input type="checkbox"/> Landscaping scheme 	See conditions 3, 4 and 5.

8. APPENDICES TO THIS REPORT

- Appendix A - Site location plan
- Appendix B - Proposed plans and elevations

This recommendation is made following careful consideration of all the issues raised through the application process and thorough discussion with the applicants. The Case Officer has sought solutions to these issues where possible to secure a development that improves the economic, social and environmental conditions of the area, in accordance with NPFF.

In this case the issues have been successfully resolved.

9. CONDITIONS RECOMMENDED FOR INCLUSION IF PERMISSION IS GRANTED

1. The development hereby permitted shall be commenced within three years from the date of this permission.
Reason: To accord with the provisions of Section 91 of the Town and Country Planning Act 1990 (as amended).
2. Prior to the construction of the dwelling and garage hereby approved details of the materials to be used shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out and maintained in accordance with the approved details.

Reason: In the interests of the visual amenities of the area. Relevant Policy DG1 and NP/DG3 of the Neighbourhood Plan

3. No works or development shall take place until an Arboricultural Method Statement and Tree Protection Plan specific to this scheme, has been submitted and approved in writing by the Local Planning Authority. The Tree Protection Plan and Arboricultural Method Statement shall be written in accordance with, and address sections 5.5, 6.1, 6.2, 6.3 and 7 of British Standard 5837:2012 Trees in relation to design, demolition and construction - recommendations. Nothing shall be stored or placed in any area in accordance with this condition and the ground levels within those areas shall not be altered, nor shall any excavation be made, without the prior written approval of the Local Planning Authority. Thereafter the works shall be carried out in accordance with the approved details until completion of the development.
Reason: To protect trees which contribute to the visual amenities of the site and surrounding area. Relevant Policies - Local Plan DG1, N6.
4. No tree or hedgerow shown to be retained in the approved plans shall be cut down, uprooted or destroyed, nor shall any retained tree be lopped or topped other than in accordance with the approved plans and particulars and without the written approval of the Local Planning Authority, until five years from the date of occupation of the building for its permitted use. Any topping or lopping approved shall be carried out in accordance with British Standard 3998 Tree work. If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted in the immediate vicinity and that tree shall be of the size and species, and shall be planted at such time, as specified by the Local Planning Authority.
Reason: In the interests of the visual amenities of the area. Relevant Policies - Local Plan DG1, N6.
5. No development shall take place until full details of both hard and soft landscape works, have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved within the first planting season following the substantial completion of the development and retained thereafter in accordance with the approved details. If within a period of five years from the date of planting of any tree or shrub shown on the approved landscaping plan, that tree or shrub, or any tree or shrub planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes seriously damaged or defective, another tree or shrub of the same species and size as that originally planted shall be planted in the immediate vicinity, unless the Local Planning Authority gives its written consent to any variation.
Reason: To ensure a form of development that maintains, and contributes positively to, the character and appearance of the area. Relevant Policies - Local Plan DG1.
6. Prior to the construction of the dwelling and garage hereby approved, details of all finished slab levels and finished floor levels in relation to ground level (against OD Newlyn) shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out and maintained in accordance with the approved details.
Reason: In the interest of the visual amenities of the area. Relevant Policy Local Plan DG1.
7. The window(s) in the side (west) elevation of the dwelling shall be of a permanently fixed, non-opening design, with the exception of an opening toplight that is a minimum of 1.7m above the finished internal floor level, and fitted with obscure glass to level 3 or above and the window shall not be altered without the prior written approval of the Local Planning Authority. No further window(s) shall be inserted at first floor level or above in the side elevations of the dwelling without the prior written approval of the Local Planning Authority.
Reason: To prevent overlooking and loss of privacy to neighbouring occupiers. Relevant Policies - Local Plan H11.
8. No part of the development shall be occupied until vehicle parking and turning space has been provided, surfaced and marked out in accordance with the approved drawing. The space approved shall be kept available for parking and turning in association with the development.
Reason: To ensure that the development is provided with adequate parking facilities in order to reduce the likelihood of roadside parking which could be detrimental to the free flow of traffic and to highway safety, and to facilitate vehicles entering and leaving the highway in forward gear. Relevant Policies - Local Plan P4, DG1.

9. Irrespective of the provisions of the Town & Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), the garage accommodation on the site shall be kept available for the parking of vehicles associated with the development at all times.
Reason: To ensure that the development is provided with adequate parking facilities in order to reduce the likelihood of roadside parking which could be detrimental to the free flow of traffic and to highway safety. Relevant Policies - Local Plan P4, DG1.
10. No part of the development shall be commenced until visibility splays of 25 metres to the left by 20 metres to the right have been provided at 2.4 metres. All dimensions are to be measured along the edge of the driveway and the back of footway from their point of intersection. The areas within these splays shall be kept free of all obstructions to visibility over a height of 0.6 metres above carriageway level.
Reason: In the interests of highway safety. Relevant Policies - Local Plan T5.
11. Irrespective of the provisions of Classes A and E of part 1 of Schedule 2 of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification) no enlargement, improvement or any other alteration (including the erection of any ancillary building within the curtilage) of or to any dwelling house the subject of this permission shall be carried out without planning permission having first been obtained from the Local Planning Authority.
Reason: The prominence of the site requires strict control over the form of any additional development which may be proposed. Relevant Policies - Local Plan H11, DG1, and policies NP/DG1 and NP/DG2 of the Neighbourhood Plan.
12. The measures set out in section 6 of the Design and Access Statement accompanying the application shall be implemented in accordance with the statement prior to the first occupation of any unit, unless otherwise agreed in writing by the Local Planning Authority prior to the commencement of the development.
Reason: To ensure that measures to make the development sustainable and efficient in the use of energy, water and materials are included in the development.
13. The development hereby permitted shall be carried out in accordance with the approved plans listed below.
Reason: To ensure that the development is carried out in accordance with the approved particulars and plans.

Informatives

1. The attention of the applicant is drawn to the Berkshire Act 1986, Part II, Clause 9, which enables the Highway Authority to recover the costs of repairing damage to the footway or grass verge arising during building operations.
2. The attention of the applicant is drawn to Section 59 of the Highways Act 1980 which enables the Highway Authority to recover expenses due to extraordinary traffic.
3. Any incidental works affecting the adjoining highway shall be approved by, and a licence obtained from The Streetcare Services Manager at Tinkers Lane Depot Tinkers Lane Windsor SL4 4LR tel: 01628 796801 at least 4 weeks before any development is due to commence.
4. No builder's materials, plant or vehicles related to the implementation of the development should be parked/stored on the public highway so as to cause an obstruction at any time.

Appendix A – Site plan

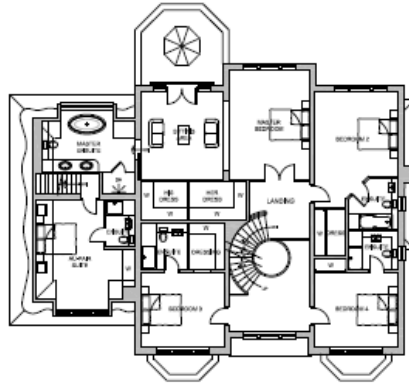


Site location

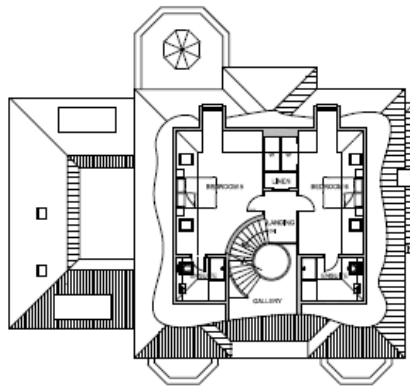




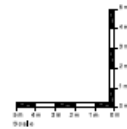
GROUND FLOOR



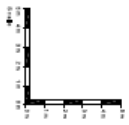
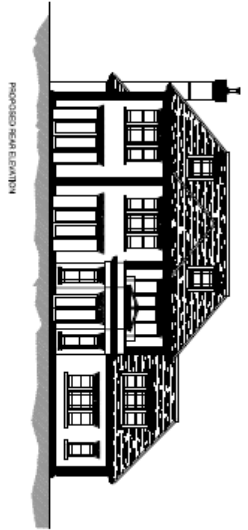
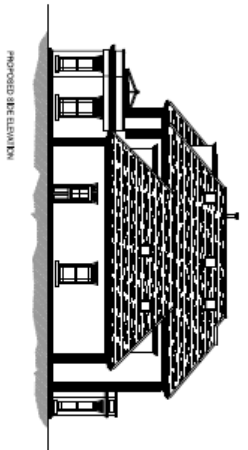
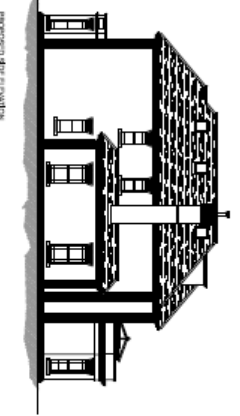
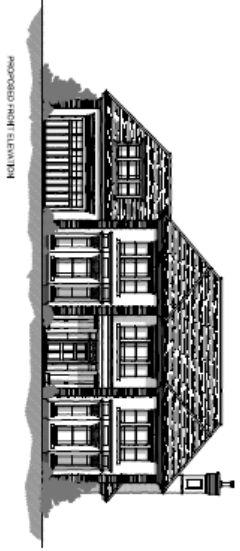
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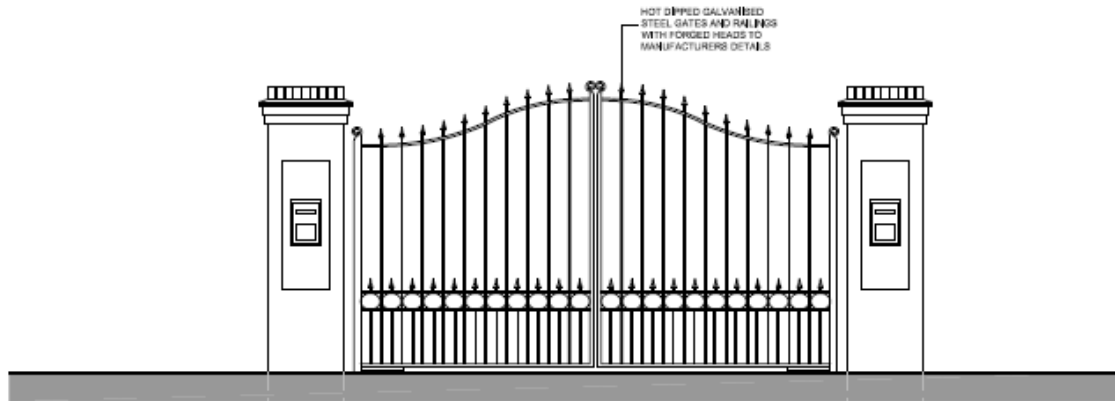
SECOND FLOOR



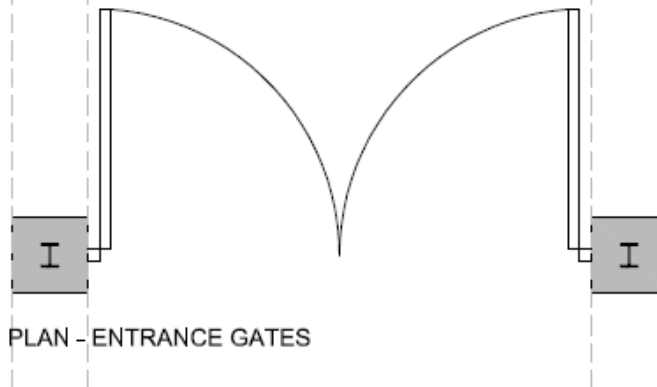
PLANNING ISSUE		
 ASCOT DESIGN Timeless architecture <small>Head Office: 10000 Old Derby Road, Derby, Derbyshire DE22 9JF Tel: 01332 353535 Fax: 01332 353536 www.ascotdesign.com</small>		
Dr & Mrs, GUNTHER		
2 FR TREE CLOSE, ASCOT, BERKSHIRE SL5 9LJ		
PROPOSED FLOOR PLANS		
1:100 @ A1	APR-18	SF
P1216 - 203		
<small>Prepared by: Dr. Gunther and Mrs. Gunther Checked by: Dr. Gunther and Mrs. Gunther Drawn by: Dr. Gunther and Mrs. Gunther</small>		



PLANNING ISSUE	
 ASCOT DESIGN Timeless Architecture <small>11050 81st Avenue, Suite 100, Richmond, BC V6V 2G9 Tel: 604-273-1100 Fax: 604-273-1101</small>	
Dr. & Mrs. GUNTHER <small>2115 16th Street, West</small>	
2 FUR TREE CLOSE, ASBOLT, BRITISH COLUMBIA	
PROPOSED ELEVATIONS	
11050 81st Tel: 604-273-1100	11050 81st Fax: 604-273-1101
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ELEVATION - ENTRANCE GATES



PLAN - ENTRANCE GATES



Scale 1:50

 <p>ASCOT DESIGN Timeless architecture</p> <p><small>Ascot Design Ltd, Berlidge House 35-37 High Street, Ascot, Berkshire, SL5 7HY Tel: 01344 296000 Fax: 01344 299333 Email: info@ascotdesign.com www.ascotdesign.co.uk</small></p> <p><small>The copyright in this document and design is confidential and the property of Ascot Design Limited</small></p>	<p>PLANNING ISSUE</p>	<p>Rev: Etc: Date:</p>	
	<p>Dr & Mrs GUNTHER</p>	<p>PROPOSED GATES</p>	
	<p>2 FIR TREE CLOSE, ASCOT, BERKSHIRE SL5 9LJ</p>	<p>Scale: 1:1250 @ A4</p>	<p>Date: APR-16</p>
		<p>Drawing No: 15 - P1216 - 206</p>	<p>Rev:</p>